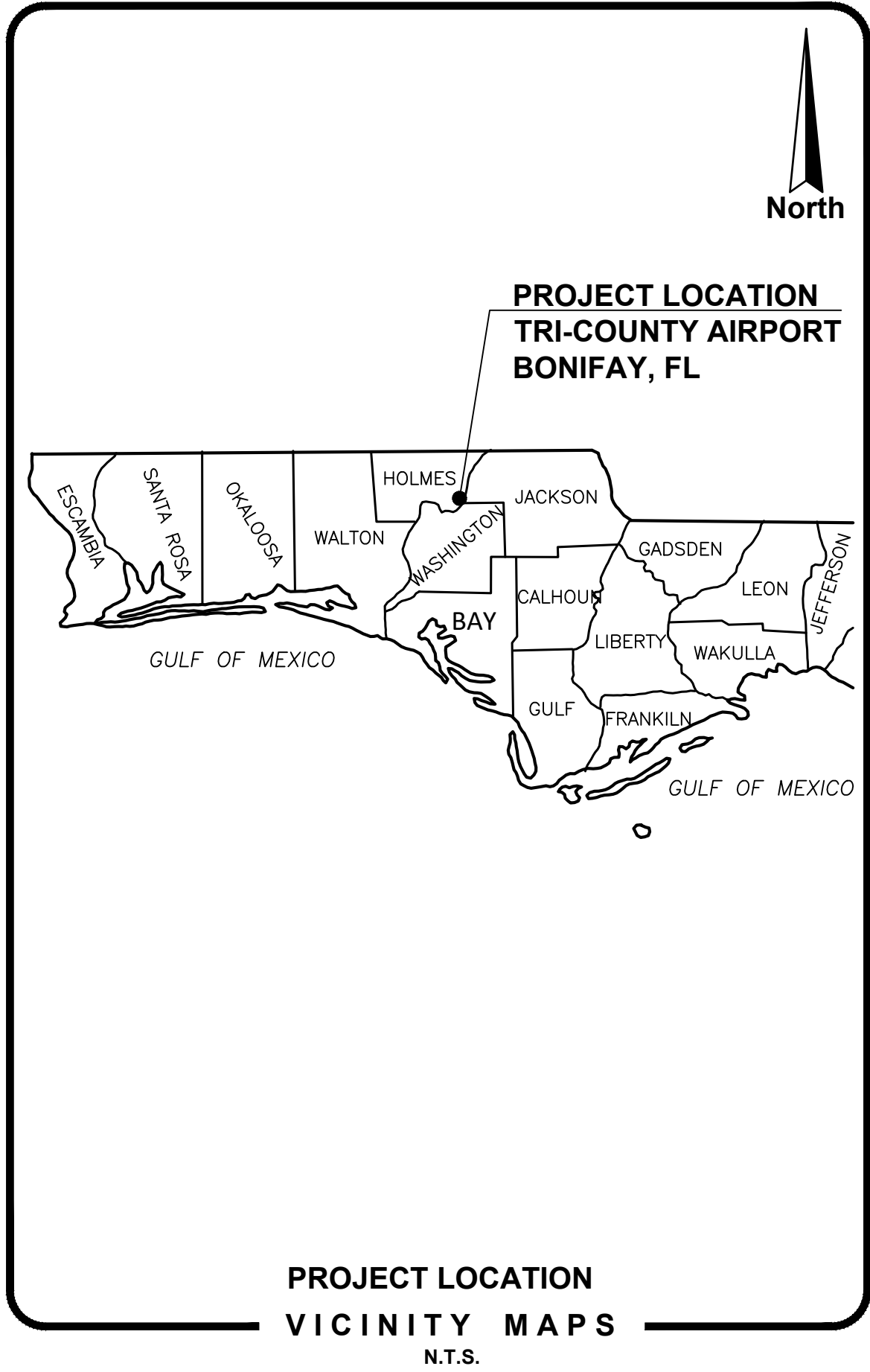


DESIGN DRAWINGS FOR:

# TERMINAL ACCESS & SECURITY IMPROVEMENTS

TRI-COUNTY AIRPORT  
BONIFAY, FLORIDA



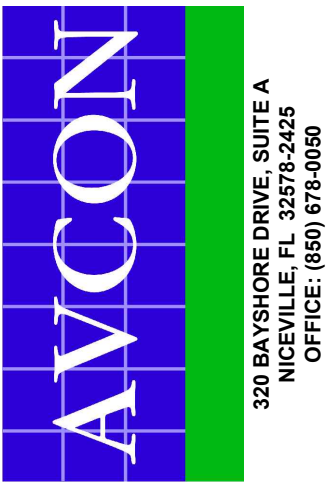
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C-11	PYRAMAT DETAILS

**CLIENT:**  
TRI-COUNTY AIRPORT AUTHORITY  
P.O. BOX 756  
BONIFAY, FLORIDA 32425  
TELEPHONE: (850) 547-6519

**TRI-COUNTY AIRPORT AUTHORITY BOARD OF DIRECTORS:**  
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FRED PROVOST  
ROY BROOKS  
THOMAS LEUENBERGER  
TIFFANY WILSON GARLING  
LOGAN CHAPPELL  
JIM TOWN  
ALAN WILLIAMS  
ROSS STATHAM  
RICK D. KERR

**ENGINEER:**  
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VIRGIL C. "LEE" LEWIS, P.E.  
FL LICENSE NO.: 50619  
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AUTHORIZATION NO. 5057

REVISION		DATE	BY

**COVER SHEET**  
**60% DESIGN DRAWINGS**

**TERMINAL ACCESS  
& SECURITY  
IMPROVEMENTS**  
PREPARED FOR  
TRI-COUNTY AIRPORT AUTHORITY

DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
DATE: FEBRUARY, 2021

**SHEET NUMBER**  
**G-01**

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SUMMARY OF QUANTITIES

ITEM NUMBER	BID ITEM	ITEM DESCRIPTION	UNIT	QUANTITY
1	101-1	MOBILIZATION	LS	1
2	102-1	MAINTENANCE OF TRAFFIC	LS	1
3	104-1	PREVENTION, CONTROL, AND ABATEMENT OF EROSION AND WATER POLLUTION	LS	1
4	110-1	CLEARING AND GRUBBING	LS	1
5	110-2	MISCELLEANOUS DEMOLITION	LS	1
6	120-1	UNCLASSIFIED EXCAVATION AND EMBANKMENT	CY	5,000
7	160-1	12" STABILIZED SUBBASE (LBR 40)	SY	4,800
8	285-1	LIMEROCK BASE COURSE	SY	4,570
9	334-1	SUPERPAVE ASPHALTIC CONCRETE	TON	510
10	425-1	FDOT TYPE 'C' DBI	EA	1
11	425-2	FDOT TYPE 'F' DBI	EA	1
12	425-3	FDOT TYPE 'G' DBI	EA	1
13	425-4	FDOT TYPE 4 CURB INLET	EA	1
14	430-1	18" ADS, N-12 PIPE	LF	160
15	430-2	24" ADS, N-12 PIPE	LF	200
16	520-1	RIBBON CURB	LF	2,100
17	520-2	F-CURB	LF	125
17	522-1	CONCRETE SIDEWALK	SY	150
18	550-1	FENCE IMPROVEMENTS	LF	325
19	550-2	24' SLIDING GATE WITH GATE OPERATOR AND KEYPAD, COMPLETE	EA	1
20	550-3	24' WIDE DOUBLE SWING GATE	EA	1
21	570-1	SODDING	SY	5,250
22	700-1	TRAFFIC CONTROL SIGNS	LS	1
23	710-1	PAVEMENT MARKING	SF	1,150
24	985-1	PYRAMAT EROSION PROTECTION	SY	60
25	LS-1	LANDSCAPING	LS	1
26	LT-1	AREA LIGHTING, COMPLETE	LS	1
27	SIGN-1	AREA SIGNAGE, COMPLETE	LS	1
28	WH-1	WHEEL STOPS	EA	57

TERMINAL ACCESS  
& SECURITY  
IMPROVEMENTS  
PREPARED FOR  
TRI-COUNTY AIRPORT AUTHORITY

DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
DATE: FEBRUARY, 2021

SHEET NUMBER  
G-02

SUMMARY OF  
QUANTITIES  
60% DESIGN DRAWINGS

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VIRGIL C. "LEE" LEWIS, P.E.  
FL LICENSE NO.: 50819  
FBPR CERTIFICATE OF  
AUTHORIZATION NO. 5057



TRI-COUNTY AIRPORT  
AUTHORITY  
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32425



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GENERAL NOTES

1. UNLESS OTHERWISE SPECIFIED, ALL WORK SHALL BE PERFORMED CONSISTENT WITH THE FOLLOWING SPECIFICATIONS: HOLMES COUNTY, FAA, AND FDOT.
2. THIS DESIGN HAS BEEN BASED UPON FIELD SURVEY PREPARED BY SOUTHEASTERN SURVEYING AND MAPPING CORPORATION. MAKES NO ASSURANCES REGARDING THE ACCURACY OF SUCH SURVEY OR GEOTECHNICAL EVALUATION.
3. ALL QUALITY CONTROL TESTING WILL BE THE RESPONSIBILITY OF THE CONTRACTOR AS SPECIFIED IN THE SPECIFICATIONS.
4. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL QUALITY ACCEPTANCE TESTING REQUIRED IN THE CONTRACT SPECIFICATIONS. THE ENGINEER RESERVES THE RIGHT TO CONDUCT QUALITY ACCEPTANCE TESTING TO CHECK THE CONTRACTOR'S TEST RESULTS.
5. THE CONTRACTOR SHALL PREPARE A WRITTEN QUALITY CONTROL / QUALITY ACCEPTANCE PLAN THAT DESCRIBES THE CONTRACTOR QUALITY CONTROL PROGRAM AND THE CONTRACTOR'S QUALITY ACCEPTANCE TESTING REQUIREMENTS. THIS WRITTEN PLAN MUST BE SUBMITTED PRIOR TO ISSUANCE OF THE NOTICE-TO-PROCEED.
6. THE PROJECT PAY ITEMS ARE PROVIDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL WORK NOT IDENTIFIED UNDER A SPECIFIC PAY ITEM SHALL BE CONSIDERED REQUIRED AND IS INCIDENTAL TO THE COST OF THE PROJECT PAY ITEMS PROVIDED.
7. CONTRACTOR SHALL PROTECT ALL EXISTING LANDSCAPING, SIDEWALKS, PAVEMENTS, CURBS, SEEDING, AND SOD NOT SPECIFIED FOR REMOVAL IN THESE PLANS. ANY DAMAGE TO THE EXISTING IMPROVEMENTS SHALL BE RESTORED BY THE CONTRACTOR AT NO COST TO THE OWNER, UNLESS OTHERWISE SPECIFIED HEREIN.
8. CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH THE SITE, INCLUDING ALL SURFACE AND SUB-SURFACE CONDITIONS, THE WORK REQUIRED AND ALL OTHER CONDITIONS THAT MAY AFFECT THE SUCCESSFUL COMPLETION OF THE JOB PRIOR TO COMMENCEMENT OF WORK.
9. THE CONTRACTOR SHALL GIVE ALL NOTICES AND COMPLY WITH ALL LAWS, ORDINANCES, RULES, REGULATIONS AND PERMIT CONDITIONS BEARING ON THE CONDUCT OF THE WORK, AS DRAWN AND SPECIFIED. IF THE CONTRACTOR OBSERVES THAT THE DRAWINGS AND SPECIFICATIONS ARE AT VARIANCE THEREWITH, HE SHALL PROMPTLY NOTIFY THE ENGINEER, IN WRITING, AND ANY NECESSARY CHANGES SHALL BE ADJUSTED, AS PROVIDED IN THE AGREEMENT FOR CHANGES IN THE WORK.
10. THE CONTRACTOR SHALL BE RESPONSIBLE TO THE OWNER AND THE ENGINEER FOR THE ACTS AND OMISSIONS OF CONTRACTOR'S EMPLOYEES AND ALL HIS SUBCONTRACTORS AND THEIR AGENTS AND EMPLOYEES AND OTHER PERSONS PERFORMING ANY OF THE WORK UNDER A CONTRACT WITH THE CONTRACTOR.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAKING ALL NECESSARY ARRANGEMENTS WITH GOVERNMENTAL DEPARTMENTS, PUBLIC UTILITIES, PUBLIC CARRIERS, SERVICE COMPANIES, AND CORPORATIONS OWNING OR CONTROLLING ROADWAYS, WATER, SEWER, GAS, ELECTRICAL, AND TELEPHONE FACILITIES SUCH AS PAVEMENTS, PIPING, WIRES, CABLES, CONDUITS, POLES, GUYS, OR OTHER SIMILAR FACILITIES, INCLUDING INCIDENTAL STRUCTURES CONNECTED THEREWITH THAT ARE ENCOUNTERED IN THE WORK IN ORDER THAT SUCH ITEMS MAY BE PROPERLY SUPPORTED, PROTECTED OR LOCATED.
12. UNLESS OTHERWISE SPECIFIED IN THE GENERAL CONDITIONS, ALL CONSTRUCTION IS TO BE GOVERNED BY THE PLANS, APPLICABLE PERMITS, AND SPECIFICATIONS HEREIN, AND ALL APPLICABLE FEDERAL, STATE AND LOCAL BUILDING AND SAFETY CODES, SPECIFICATIONS, LAWS AND ORDINANCES. TO INCLUDE BUT NOT LIMITED TO THE FAA, THE FDOT, THE FLORIDA BUILDING CODE, AND THE BAY COUNTY CODES.
13. PRIOR TO PERFORMING ANY WORK WITHIN ANY PUBLIC RIGHT-OF-WAY, CONTRACTOR SHALL DEVELOP AND IMPLEMENT A TRAFFIC CONTROL PLAN CONSISTENT WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" PUBLISHED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION.
14. PRIOR TO PERFORMING ANY WORK WITHIN ANY UTILITY RIGHT-OF-WAY, CONTRACTOR SHALL OBTAIN AUTHORIZATION AND PERMIT FROM JURISDICTION RESPONSIBLE FOR SUCH RIGHT-OF-WAY.
15. IN THE EVENT THE CONTRACTOR DISCOVERS ANY ERRORS OR OMISSIONS IN THE PLANS, HE SHALL IMMEDIATELY NOTIFY THE ENGINEER.
16. CONTRACTOR SHALL PRESERVE AND PROTECT ALL PERMANENT REFERENCE MONUMENTS, PERMANENT CONTROL POINTS, PERMANENT BENCH MARKS AND PROPERTY CORNERS. IN THE EVENT THE MONUMENTS, POINTS OR MARKERS ARE DISTURBED THE CONTRACTOR SHALL EMPLOY A FLORIDA REGISTERED LAND SURVEYOR TO RESET OR REPLACE THEM. CANCELLATION OF THE RESET OR REPLACEMENT SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
17. THE OWNER, OWNER'S AGENT AND INSPECTORS OF APPLICABLE GOVERNMENT JURISDICTIONS, SHALL AT ALL TIMES HAVE ACCESS TO THE WORK WHEREVER AND WHENEVER IT IS IN PREPARATION OR PROGRESS; AND THE CONTRACTOR SHALL PROVIDE PROPER FACILITIES FOR SUCH ACCESS AND FOR THE INSPECTION.
18. IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE ALL REASONABLE AND PRUDENT PRECAUTIONS TO INSURE THAT ALL COMPLETED WORK, MATERIALS AND EQUIPMENT STORED ON SITE ARE SAFE AND SECURED FROM UNAUTHORIZED ACCESS OR USE UNTIL SUCH TIME THAT THE OWNER TAKES WRITTEN OWNERSHIP OF THE COMPLETED PROJECT. SUCH PRECAUTIONS MAY INCLUDE INSTALLATION OF SIGNS, FENCES, OR POSTING OF SECURITY GUARDS.
19. CONTRACTOR SHALL, AT ALL TIMES, UTILIZE ALL NORMALLY ACCEPTED AND REASONABLY EXPECTED SAFETY PRACTICES AND COMPLY WITH ALL FEDERAL, STATE AND LOCAL REGULATIONS, ORDINANCES AND GUIDELINES PERTAINING TO SAFE UTILIZATION OF EQUIPMENT OR MATERIALS AS PUBLISHED BY THE MANUFACTURER.
20. PRIOR TO INITIATING ANY EXCAVATION (INCLUDING BUT NOT LIMITED TO TUNNELS, DITCHES, STORMWATER PONDS, CANALS) CONTRACTOR SHALL INSTALL FENCES AND TAKE ALL OTHER REASONABLE AND PRUDENT STEPS TO ENSURE THAT ACCESS TO EXCAVATION BY UNAUTHORIZED PERSONNEL IS PREVENTED.
21. THE CONTRACTOR SHALL TAKE ALL REASONABLE PRECAUTIONS FOR THE SAFETY OF, AND SHALL PROVIDE ALL REASONABLE PROTECTION TO PREVENT DAMAGE, INJURY OR LOSS TO:

21.1. ALL EMPLOYEES ON THE WORK SITE AND ALL OTHER PERSONS WHO MAY BE AFFECTED THEREBY;

21.2. ALL WORK AND ALL MATERIALS AND EQUIPMENT TO BE INCORPORATED THEREIN, WHETHER IN STORAGE ON OR OFF THE SITE, UNDER THE CARE, CUSTODY OR CONTROL OF THE CONTRACTOR OR ANY OF ITS SUBCONTRACTORS;

21.3. ANY OTHER PROPERTY AT THE SITE OR ADJACENT THERETO, INCLUDING TREES, SHRUBS, LAWNS, WALKS, PAVEMENTS, ROADWAY, STRUCTURES AND UTILITIES NOT DESIGNATED FOR DEMOLITION IN THE COURSE OF CONSTRUCTION.
22. CONTRACTOR SHALL MAINTAIN PUBLIC ACCESS ON MAIN AIRPORT ENTRANCE ACCESS ROAD, ON GENERAL AVIATION ACCESS ROAD, AND ON THE AIRSIDE AIRPORT ACCESS ROAD AT ALL TIMES.

23. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE SAFETY CODES AND WITH ALL APPLICABLE LAWS, ORDINANCES, RULES, REGULATIONS AND LAWFUL ORDERS OF ANY PUBLIC, QUASI-PUBLIC OR OTHER AUTHORITY HAVING JURISDICTION FOR THE SAFETY OF PERSONS OR PROPERTY OR FOR THEIR PROTECTION AGAINST DAMAGE, INJURY OR LOSS, OR DESIGNED TO PROTECT THE ENVIRONMENT. THE CONTRACTOR SHALL ERECT AND MAINTAIN, AS REQUIRED BY EXISTING CONDITIONS AND PROGRESS OF THE WORK, ALL REASONABLE SAFEGUARDS FOR SAFETY AND PROTECTION, INCLUDING POSTING DANGER SIGNS AND OTHER WARNINGS AGAINST HAZARDS, PROMULGATING SAFETY REGULATIONS AND NOTIFYING OWNERS AND USERS OF ADJACENT UTILITIES OF THE EXISTENCE OF HAZARDS AND OF THE SAFETY REGULATIONS.
24. ALL DAMAGE OR LOSS TO ANY PROPERTY REFERRED TO IN CLAUSES 21.2 AND 21.3 CAUSED IN WHOLE OR IN PART BY THE CONTRACTOR, A SUBCONTRACTOR, OR BY ANYONE FOR WHOSE ACTS ANY OF THEM MAY BE LIABLE, SHALL BE REMEDIED BY THE CONTRACTOR, EXCEPT DAMAGE OR LOSS PROPERLY ATTRIBUTABLE SOLELY TO THE ACTS OR OMISSIONS OF THE OWNER, OR THE ENGINEER OR ANYONE EMPLOYED BY THEM, OR FOR WHOSE ACTS ANY OF THEM MAY BE LIABLE, AND NOT PROPERLY ATTRIBUTABLE IN WHOLE OR IN PART, TO THE FAULT OR NEGLIGENCE OF THE CONTRACTOR.
25. UNTIL FINAL ACCEPTANCE OF THE WORK BY OWNER, THE CONTRACTOR SHALL HAVE THE CHARGE AND CARE OF AND SHALL BEAR THE RISK OF INJURY OR DAMAGE, LOSS OR EXPENSE TO ANY PART THEREOF, OR TO ANY MATERIALS STORED ON SITE, BY THE ACTION OF THE ELEMENTS OR FROM ANY OTHER CAUSE WHETHER ARISING FROM THE EXECUTION OR NON-EXECUTION OF THE WORK. THE CONTRACTOR SHALL REBUILD, REPAIR, RESTORE AND MAKE GOOD ALL INJURIES OR DAMAGES TO ANY PORTION OF THE WORK OCCASIONED BY ANY OF THE ABOVE CAUSES BEFORE FINAL ACCEPTANCE AND SHALL BEAR THE EXPENSES THEREOF.
26. THOSE PARTS OF WORK IN PLACE WHICH ARE SUBJECT TO DAMAGE BECAUSE OF OPERATIONS BEING CARRIED ON ADJACENT THERETO SHALL BE COVERED, BOARDED UP OR SUBSTANTIALLY ENCLOSED WITH ADEQUATE PROTECTION BY THE CONTRACTOR AT CONTRACTOR'S EXPENSE.
27. ADEQUATE TRAFFIC CONTROL, BARRICADES AND FLAGMAN SERVICES SHALL BE FURNISHED AND MAINTAINED BY THE CONTRACTOR AT ALL POINTS WHERE CONVEYING EQUIPMENT ENGAGED ON THE WORK REGULARLY ENTERS ONTO OR CROSSES TRAFFIC-CARRYING ROADS.
28. THE CONTRACTOR SHALL COMPLY IN EVERY RESPECT WITH THE FEDERAL OCCUPATIONAL HEALTH AND SAFETY ACT OF 1970 AND ALL RULES AND REGULATIONS NOW OR HEREAFTER IN EFFECT UNDER SAID ACT, AND THE CONTRACTOR FURTHER AGREES TO COMPLY WITH ANY AND ALL APPLICABLE STATE LAWS AND REGULATIONS PERTAINING TO JOB SAFETY AND HEALTH.
29. THE CONTRACTOR SHALL PROTECT AND KEEP OWNER (INCLUDING THEIR AGENTS AND EMPLOYEES) FREE AND HARMLESS FROM ANY AND ALL LIABILITY, PUBLIC OR PRIVATE, PENALTIES, CONTRACTUAL OR OTHERWISE, LOSSES, DAMAGES, COSTS, ATTORNEY'S FEES, EXPENSES, CAUSES OF ACTION, CLAIMS OR JUDGMENTS RESULTING FROM THE FEDERAL OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970 AS AMENDED OR ANY RULE OR REGULATION PROMULGATED THEREUNDER OR OF ANY STATE LAWS OR REGULATIONS PERTAINING TO JOB SAFETY AND HEALTH ARISING OUT OF OR IN ANY WAY CONNECTED WITH THE PERFORMANCE OF WORK OR WORK TO BE PERFORMED UNDER THIS CONTRACT, AND CONTRACTOR SHALL INDEMNIFY OWNER FROM ANY SUCH CLAIMS, PENALTIES, SUITS OR ACTIONS, PUBLIC OR PRIVATE, ADMINISTRATIVE OR JUDICIAL, INCLUDING ATTORNEY'S FEES PAID OR INCURRED BY OR ON BEHALF OF OWNER, JOINTLY OR SEVERALLY, AND/OR THEIR AGENTS AND EMPLOYEES. THE CONTRACTOR FURTHER AGREES, IN THE EVENT OF A CLAIMED VIOLATION OF ANY FEDERAL OR STATE SAFETY AND HEALTH LAW OR REGULATION ARISING OUT OF OR IN ANY WAY CONNECTED WITH THE PERFORMANCE OF WORK OR WORK TO BE PERFORMED UNDER THIS CONTRACT, OWNER MAY IMMEDIATELY TAKE WHATEVER ACTION IS DEEMED NECESSARY BY OWNER TO REMEDY THE CLAIMED VIOLATION, ANY AND ALL COSTS OR EXPENSES PAID OR INCURRED BY OWNER IN TAKING SUCH ACTION SHALL BE BORNE BY CONTRACTOR, AND CONTRACTOR AGREES TO PROTECT, HOLD HARMLESS AND INDEMNIFY OWNER AGAINST ANY AND ALL SUCH COSTS OR EXPENSES.
30. ALL WORK PERFORMED UNDER THE CONTRACT, AND ALL EQUIPMENT, APPLIANCES, TOOLS AND LIKE ITEMS USED IN THE WORK SHALL CONFORM TO APPLICABLE SAFETY CODES AND REGULATIONS OF ANY PUBLIC OR OTHER AUTHORITY HAVING JURISDICTION. IN THE EVENT OF CONFLICTING REQUIREMENTS, THE MORE STRINGENT INTERPRETATION OR REGULATION SHALL GOVERN.
31. THE CONTRACTOR SHALL DEVELOP AND IMPLEMENT AN EROSION CONTROL PLAN TO MINIMIZE EROSION AND ENSURE FUNCTIONING OF STORMWATER MANAGEMENT SYSTEM UPON COMPLETION OF CONSTRUCTION. EROSION CONTROL PLAN SHALL INCLUDE PROVISIONS TO STABILIZE DISTURBED AREAS WITHIN 14 CALENDAR DAYS OF THE DISTURBANCE WITH A WRITTEN LOG OF THE EVENTS. CONTRACTOR SHALL SUBMIT EROSION CONTROL PLAN TO ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
32. CONTRACTOR AND ITS SUBCONTRACTORS SHALL USE, HANDLE, TRANSPORT, AND DISPOSE OF ALL HAZARDOUS MATERIALS (AS DEFINED PARAGRAPH 40.) IN COMPLIANCE WITH ALL PRESENT FEDERAL, STATE AND LOCAL ENVIRONMENTAL HEALTH OR SAFETY LAW, INCLUDING, BUT NOT LIMITED TO, ALL SUCH STATUTES, REGULATIONS, RULES, ORDINANCES, CODES, AND RULES OF COMMON LAW.
33. CONTRACTOR FURTHER AGREES THAT CONTRACTOR AND ITS SUBCONTRACTORS SHALL NOT CAUSE THE DISCHARGE, RELEASE OR DISPOSAL OF ANY HAZARDOUS MATERIAL CREATED BY ITS WORK ON OR ABOUT THE JOB SITE. IN THE EVENT OF ANY SPILL, RELEASE OR ANY OTHER REPORTABLE OCCURRENCE, CONTRACTOR SHALL NOTIFY THE APPROPRIATE GOVERNMENTAL AGENCY AND SHALL TAKE SUCH ACTION AS MAY BE NECESSARY TO MINIMIZE THE DELETERIOUS EFFECT OF SUCH SPILL ON PERSONS OR PROPERTY.
34. CONTRACTOR AND ITS SUBCONTRACTORS SHALL, UPON COMPLETION OF PERFORMANCE OF ALL DUTIES UNDER THIS CONTRACT, REMOVE ALL SUPPLIES, MATERIALS, AND WASTE CONTAINING AND HAZARDOUS MATERIAL FROM THE JOB SITE. CONTRACTOR SHALL BEAR FULL FINANCIAL RESPONSIBILITY, AS BETWEEN THE PARTIES OF THIS CONTRACT, FOR THE COMPLIANCE OF CONTRACTOR AND ITS SUBCONTRACTORS WITH THE PROVISIONS OF THIS PARAGRAPH.
35. CONTRACTOR AGREES TO INDEMNIFY, DEFEND, PROTECT AND HOLD THE OWNER HARMLESS FROM AND AGAINST ANY CLAIMS INCLUDING, WITHOUT LIMITATION, ACTUAL ATTORNEY'S FEES AND ANY COSTS OF INVESTIGATION, SOILS TESTING, GOVERNMENTAL APPROVALS, REMEDIATION AND CLEAN-UP ARISING OUT OF OR IN ANY WAY CONNECTED WITH THE FAILURE OF CONTRACTOR OR ITS SUBCONTRACTORS, OR THEIR AGENTS, EMPLOYEES, OFFICERS, OR REPRESENTATIVES, TO COMPLY WITH THE TERMS OF THIS ARTICLE.
36. SHOULD CONTRACTOR OR ITS SUBCONTRACTORS DISCHARGE, RELEASE OR DISPOSE OF ANY HAZARDOUS MATERIAL ON OR ABOUT THE JOB SITE IN VIOLATION OF REGULATIONS, CONTRACTOR SHALL IMMEDIATELY SO INFORM OWNER IN WRITING.
37. IN THE EVENT OF ANY SPILL, RELEASE OR ANY OTHER REPORTABLE OCCURRENCE, CONTRACTOR SHALL NOTIFY THE APPROPRIATE GOVERNMENTAL AGENCY AND SHALL TAKE SUCH ACTION AS MAY BE NECESSARY TO MINIMIZE THE DELETERIOUS EFFECT OF SUCH SPILL ON PERSONS OR PROPERTY. IN THE EVENT CONTRACTOR OR ITS SUBCONTRACTORS ENCOUNTER ON THE PREMISES ANY PIPELINE, UNDERGROUND STORAGE TANK OR OTHER CONTAINER, OF ANY KIND, THAT MAY CONTAIN A HAZARDOUS MATERIAL, OR ENCOUNTER MATERIAL REASONABLY BELIEVED TO BE A HAZARDOUS MATERIAL, CONTRACTOR SHALL IMMEDIATELY STOP WORK IN THE AREA AFFECTED AND REPORT THE CONDITION TO OWNER IN WRITING.
38. IF CONTRACTOR OR ITS SUBCONTRACTORS DO NOT COMPLY WITH FEDERAL AND STATE REQUIREMENTS, OWNER MAY, BUT IS NOT OBLIGATED TO, GIVE WRITTEN NOTICE OF VIOLATION TO CONTRACTOR. SHOULD CONTRACTOR OR ITS SUBCONTRACTORS FAIL TO COMPLY WITH THE REQUIREMENTS WITHIN TWENTY-FOUR (24) HOURS FROM THE TIME OWNER ISSUES SUCH WRITTEN NOTICE OF NONCOMPLIANCE OR WITHIN THE TIME OF AN ABATEMENT PERIOD SPECIFIED BY ANY

GOVERNMENTAL AGENCY, WHICHEVER PERIOD IS SHORTER, CONTRACTOR SHALL BE IN MATERIAL DEFAULT OF THIS CONTRACT.

39. "HAZARDOUS MATERIAL" MEANS ANY SUBSTANCE: (A) THE PRESENCE OF WHICH REQUIRES INVESTIGATION OR REMEDIATION UNDER ANY PRESENT FEDERAL, STATE OR LOCAL STATUTE, REGULATION, ORDINANCE, RULE, CODE, ORDER, ACTION, POLICY OR COMMON LAW, OR (B) WHICH IS OR BECOMES DEFINED AS A "HAZARDOUS WASTE," "HAZARDOUS SUBSTANCE," POLLUTANT OR CONTAMINANT UNDER ANY PRESENT FEDERAL, STATE OR LOCAL STATUTE, REGULATION, RULE OR ORDINANCE OR AMENDMENTS THERETO INCLUDING, WITHOUT LIMITATION, THE COMPREHENSIVE ENVIRONMENTAL RESPONSE COMPENSATION AND LIABILITY ACT (42 U.S.C. SECTIONS 9601 ET SEQ.) AND/OR THE RESOURCE CONSERVATION AND RECOVERY ACT (42 U.S.C. SECTIONS 6901 ET SEQ.), OR (C) WHICH IS TOXIC, EXPLOSIVE, CORROSIVE, FLAMMABLE, INFECTIOUS, RADIOACTIVE, CARCINOGENIC, MUTAGENIC, OR OTHERWISE HAZARDOUS AND IS REGULATED BY ANY GOVERNMENTAL AUTHORITY, AGENCY, DEPARTMENT, COMMISSION, BOARD, AGENCY OR INSTRUMENTALITY OF THE UNITED STATES, THE STATE IN WHICH THE PREMISES ARE LOCATED OR ANY POLITICAL SUBDIVISION THEREOF, OR (D) THE PRESENCE OF WHICH ON THE PREMISES CAUSES OR THREATENS TO CAUSE A NUISANCE UPON THE PREMISES OR TO ADJACENT PROPERTIES OR POSES OR THREATENS TO POSE A HAZARD TO THE HEALTH OR SAFETY OF PERSONS ON OR ABOUT THE PREMISES, OR (E) WHICH CONTAINS GASOLINE, DIESEL FUEL OR OTHER PETROLEUM HYDROCARBONS, OR (F) WHICH CONTAINS POLYCHLORINATED BIPHENYLS (PCBS), ASBESTOS, LEAD OR UREA FORMALDEHYDE FOAM INSULATION.
40. THE EXISTING UTILITIES SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL FIELD LOCATE ALL EXISTING UTILITIES AS TO SIZE, LOCATION, AND ELEVATION. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY AND ALL CONFLICTS PRIOR TO BEGINNING CONSTRUCTION.
41. IF ANY TESTING, INSPECTION OR APPROVAL REVEAL DEFECTIVE WORK, CONTRACTOR SHALL NOT BE ENTITLED TO RECEIVE ANY ASSOCIATED COSTS AND THE OWNER SHALL BE ENTITLED TO DEDUCT FROM THE CONTRACT PRICE, BY ISSUING A CHANGE ORDER, OWNER'S COSTS ARISING OUT OF THE DEFECTIVE WORK, INCLUDING COSTS OF REPEATED PROCEDURES, COMPENSATION FOR OWNER AUTHORIZED REPRESENTATIVE, DESIGN ENGINEER'S SERVICES, FIELD REPRESENTATIVE SERVICES, AND OTHER RELATED COSTS.
42. ENGINEER SHALL REVIEW RED LINE (AS-BUILT) DRAWINGS MONTHLY AT ALTERNATE BI-WEEKLY JOB COORDINATION MEETINGS. THE DRAWINGS CAN BE PROVIDED BY THE OWNER'S AUTHORIZED REPRESENTATIVE OR THE CONTRACTOR. NO PERIODIC PAY REQUESTS WILL BE PROCESSED UNTIL THIS PROVISION IS MET.
43. TYPE AND HEIGHT (NOT-TO-EXCEED) OF CONSTRUCTION EQUIPMENT:  
TRUCKS (DUMP, FLATBED, PANEL, PICKUP, CONCRETE) – 35 FEET  
  
FRONT END LOADERS – 35 FEET  
DOZERS – 35 FEET  
CRANE – 50 FEET\*  
ROLLERS AND COMPACTORS – 35 FEET  
  
\*NOTE – CONSTRUCTION EQUIPMENT LOCATIONS SHALL NOT VIOLATE RUNWAY 01-19 TRANSITIONAL SURFACES AND RUNWAY APPROACH ZONE HEIGHT LIMITATIONS PER SAFETY DURING CONSTRUCTION PLAN EXCEPT UNDER SPECIAL WAIVER CONDITIONS. APPROPRIATE WAIVERS MUST BE OBTAINED BY THE OWNER FROM FAA.
44. THE FOLLOWING FAA ADVISORY CIRCULARS SHALL APPLY TO THIS PROJECT:  
AC 150/5300-13A-CHANGE 1 AIRPORT DESIGN  
AC 150/5340-18F STANDARDS FOR AIRPORT SIGN SYSTEMS  
AC 150/5340-30J DESIGN AND INSTALLATION DETAILS FOR VISUAL AIDS  
AC 150/5370-10G STANDARDS FOR SPECIFYING CONSTRUCTION OF AIRPORTS  
AC 150/5345-44K SPECIFICATION FOR RUNWAY AND TAXIWAY SIGNS  
AC 150/5340-1L STANDARD FOR AIRPORT MARKINGS  
AC 150/5345-46E SPECIFICATION FOR RUNWAY AND TAXIWAY LIGHT FIXTURES
45. CONTRACTOR SHALL ABIDE BY FEDERAL BUY AMERICAN REQUIREMENTS.

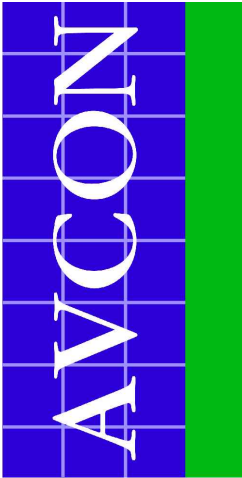
PERMITS:

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND PAYING FOR ALL NECESSARY PERMITS. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE FEDERAL, STATE, AND LOCAL REGULATIONS IN REGARD TO NOISE CONTROL, EROSION CONTROL, DUST CONTROL, WATERSHED, EMISSIONS, AND OPEN-AIR BURNING DURING CONSTRUCTION WHICH PERTAIN TO CONSTRUCTION ACTIVITIES. COPIES OF ALL PERMITS SHALL BE SUBMITTED TO THE ENGINEER FOR THEIR RECORDS.
2. AS REQUIRED UNDER ACT OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES), THE CONTRACTOR SHALL PREPARE AND SUBMIT A NOTICE OF INTENT (NOI) AND A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) TO THE FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION. A COPY OF THE NOI AND SWPPP SHALL BE SUBMITTED TO THE ENGINEER FOR THEIR RECORDS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE SWPPP CURRENT UNTIL PROJECT COMPLETION AND FINAL ACCEPTANCE OF THE WORK.

ABBREVIATIONS

BFP = BACKFLOW PREVENTER	IR = IRON ROD	PP = POWER POLE
BM = BENCH MARK	LP = IRON PIPE	PRM = PERMANENT REFERENCE MONUMENT
C/O = SEWER CLEAN OUT	LP = LIGHT POLE	RCP = REINFORCED CONCRETE PIPE
CM = CONCRETE MONUMENT	MES = MITERED END SECTION	RSO = RECLAIM WATER STUB OUT
CMP = CORRUGATED METAL PIPE	MH = MANHOLE	RV = RECLAIM WATER VALVE
CPP = CORRUGATED PLASTIC PIPE	MHD = MANHOLE-DRAINAGE	SN = SANITARY SEWER
ELEC = ELECTRIC	MHS = MANHOLE-SANITARY	SSO = SANITARY SEWER STUB OUT
ELEV = ELEVATION	MHT = MANHOLE-TELEPHONE	SV = SEWER VALVE
ES = ELECTRIC SERVICE	NAD = NORTH AMERICAN DATUM	TEL = TELEPHONE
ETP = ELECTRIC TRANSFORMER PAD	NAVD = NORTH AMERICAN VERTICAL DATUM,	USGS = U.S. GEODETIC SURVEY
FDOT = FLORIDA DEPT OF TRANSPORTATION	ADJUSTMENT OF 1988	WM = WATER METER
FFE = FINISHED FLOOR ELEVATION	NGS = NATIONAL GEODETIC SURVEY	WSO = WATER STUB OUT
FO = FIBER OPTICS	NGVD = NATIONAL GEODETIC VERTICAL DATUM,	WV = WATER VALVE
HYD = FIRE HYDRANT	ADJUSTMENT OF 1929	x.o.O = INDICATES SPOT ELEVATION
GPS = GLOBAL POSITIONING SYSTEM	PCP = PERMANENT CONTROL POINT	
ID = IDENTIFICATION	PK = PARKER-KALON BRAND OR MAG-NAIL	
INV = INVERT	(CASE HARD)	

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VIRGIL C. "LEE" LEWIS, P.E.  
FL LICENSE NO.: 50619  
FBPR CERTIFICATE OF  
AUTHORIZATION NO. 5057

NO.	DATE	REVISION	BY

GENERAL NOTES

60% DESIGN DRAWINGS

TERMINAL ACCESS  
& SECURITY  
IMPROVEMENTS

PREPARED FOR

TRI-COUNTY AIRPORT AUTHORITY

DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
DATE: FEBRUARY, 2021

SHEET NUMBER

G-03

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GENERAL NOTES:

STAGING/STORAGE AREAS:

1. THE EXACT LIMITS OF THE CONTRACTOR'S STAGING AND STORAGE AREA SHALL BE ESTABLISHED BY THE CONTRACTOR WITH THE APPROVAL OF THE ENGINEER IN THE AREAS GENERALLY AS SHOWN ON THE PLANS. ANY AND ALL REQUIRED UTILITIES FOR THE CONTRACTOR'S OPERATIONS SHALL BE ARRANGED FOR AND PAID FOR BY THE CONTRACTOR DIRECTLY WITH THE APPROPRIATE UTILITY AGENCIES. UTILITY ARRANGEMENTS SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE CONTRACTOR SHALL PROVIDE PROPER AND SANITARY TOILET FACILITIES FOR HIS/HER EMPLOYEES.
2. UPON COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL RESTORE ALL GRASSED, GRAVELED AND PAVED AREAS USED FOR STAGING AND STORAGE TO A CLEAN AND NEAT CONDITION ACCEPTABLE TO THE OWNER. THE TERRAIN SHALL BE LEFT IN A SMOOTH, WELL GROOMED, AND GRADED-TO-DRAIN CONDITION INCLUDING THE REFILLING OF ANY RUTS, HOLES, OR OTHER DEPRESSIONS OR THE LEVELING OF BERMS OR OTHER SIMILAR EMBANKMENTS AS MAY BE APPLICABLE. UPON ACCEPTANCE OF THE RESTORED STAGING OR STORAGE AREAS, THE CONTRACTOR SHALL SOD THESE AREAS IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS. NO DIRECT MEASUREMENT OR PAYMENT WILL BE MADE FOR THE CONSTRUCTION, MAINTENANCE, RESTORATION, REPAIR, AND SODDING OF STAGING AND STORAGE AREAS.
3. CONTRACTOR SHALL USE NON-GRAVEL STABILIZATION, TO MINIMIZE FOREIGN OBJECT DEBRIS DURING CONSTRUCTION ACTIVITIES AND PREVENT ALL OFFSITE SEDIMENT TRACKING. CONTRACTOR SHALL INSTALL TYPE "III" SILT FENCE 5' OUTSIDE THE PERIMETER OF THE STAGING AREA.

HAUL AND ACCESS ROADS:

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONSTRUCTING AND MAINTAINING HAUL AND ACCESS ROADS WITHIN THE LIMITS OF CONSTRUCTION, STAGING AREAS, AND BETWEEN CONSTRUCTION AREAS, AND FOR THE DUST CONTROL OF THESE ROADS. ALL COSTS ASSOCIATED WITH CONSTRUCTING AND MAINTAINING HAUL ROADS SHALL BE CONSIDERED A SUBSIDIARY OBLIGATION OF THE PROJECT AND SHALL NOT BE PAID FOR SEPARATELY.
2. ANY ADDITIONAL HAUL OR ACCESS ROADS REQUESTED BY THE CONTRACTOR FOR HIS OPERATIONS OUTSIDE THE LIMITS OF CONSTRUCTION SHALL BE CONSTRUCTED BY THE CONTRACTOR AT LOCATIONS WHERE AND IF APPROVED BY THE ENGINEER AT NO ADDITIONAL COMPENSATION.
3. THE ENGINEER RESERVES THE RIGHT TO SHIFT THE LOCATION OF CONTRACTOR HAUL AND ACCESS ROUTES AS MAY BE DEEMED NECESSARY FOR THE ORDERLY PROGRESS OF THE OVERALL AIRPORT DEVELOPMENT PROJECT AT NO ADDITIONAL COMPENSATION.
4. ANY DAMAGE TO EXISTING ROADWAYS USED AS A HAUL OR ACCESS ROUTE SHALL BE CONSIDERED THE RESPONSIBILITY OF THE CONTRACTOR AND HE SHALL PROMPTLY REPAIR ANY DAMAGED ROADWAYS TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE ENGINEER WITH NO ADDITIONAL COMPENSATION. PRIOR TO BEGINNING THE WORK, THE CONDITION OF APPLICABLE ROADWAYS SHALL BE JOINTLY INSPECTED AND AGREED UPON BY THE CONTRACTOR AND ENGINEER.
5. UPON COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL RESTORE ALL GRASSED, GRAVELED AND PAVED AREAS USED FOR HAUL ROADS TO A CLEAN AND NEAT CONDITION ACCEPTABLE TO THE OWNER. THE TERRAIN SHALL BE LEFT IN A SMOOTH, WELL GROOMED, AND GRADED-TO-DRAIN CONDITION INCLUDING THE REFILLING OF ANY HOLES OR DEPRESSIONS OR THE LEVELING OF ANY RUTS AS MAY BE APPLICABLE. THE REFILLED MATERIAL SHALL BE SUITABLE TO THE AREA BEING RESTORED (GRAVEL IF AN EXISTING GRAVEL ROAD IS BEING RESTORED, SOIL IF A TURF AREA IS BEING RESTORED, ETC.). UPON ACCEPTANCE OF A HAUL ROAD RESTORED WITHIN A PREEXISTING TURF AREA, THE CONTRACTOR SHALL SOD THESE AREAS IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS. NO DIRECT MEASUREMENT OR PAYMENT WILL BE MADE FOR THE CONSTRUCTION, MAINTENANCE, RESTORATION, REPAIR, GRAVELING, SODDING OF HAUL ROADS.
6. WORK BY OTHERS: ADDITIONAL CONTRACTORS MAY BE WORKING WITHIN THE LIMITS OF CONSTRUCTION FOR THIS PROJECT. CLOSE COORDINATION OF WORK BETWEEN THE CONTRACTOR AND OTHER WORK IN THE AREA WILL BE REQUIRED. THE CONTRACTOR SHALL COOPERATE WITH THE ENGINEER IN COORDINATING SCHEDULES IN ORDER TO MINIMIZE CONFLICTS AND COMPLETE THE PROJECTS IN A TIMELY MANNER. THE CONTRACTOR SHALL COORDINATE HIS WORK SO AS NOT TO DISRUPT OR INTERFERE WITH WORK BEING ACCOMPLISHED BY OTHER CONTRACTORS. THE ENGINEER RESERVES THE RIGHT TO ADJUST PROJECT LIMITS AS MAY BE DEEMED NECESSARY TO ACCOMMODATE ADJACENT WORK BY OTHERS. ANY SUCH NECESSARY ADJUSTMENT WHICH IMPACTS THE SCHEDULE OF THE CONTRACTOR MAY BE THE BASIS FOR A REQUEST FOR EXTRA TIME. HOWEVER, IT SHALL NOT BE THE BASIS FOR A CLAIM FOR EXTRA COSTS.

SAFETY NOTES:

1. CONTRACTOR SHALL YIELD TO AIRCRAFT AT ALL TIMES. CONTRACTOR SHALL YIELD RIGHT-OF-WAY IN AREAS LEASED BY OTHERS AT ALL TIMES.
2. EQUIPMENT HEIGHT SHALL NOT EXCEED 35 FEET ABOVE GROUND LEVEL UNLESS SPECIFICALLY REQUESTED BY CONTRACTOR AND APPROVED BY THE AIRPORT.
3. RESTRICTIONS INDICATED ON THIS SHEET SHALL APPLY AT ALL TIMES.
4. BARRICADE PLACEMENT TO BE COORDINATED WITH OWNER'S FIELD REPRESENTATIVE, 2 FEET FROM THE EDGE OF ALL EXISTING AFFECTED PAVEMENTS, UNLESS OTHERWISE SHOWN. BARRICADES SHALL BE CONTINUOUS (NO SPACES).
5. BARRICADE SECTIONS SHALL BE WHITE WITH ORANGE REFLECTIVE MEDIA. ALL INCIDENTAL CONNECTORS, SPACERS, SPLICE PLATES, ETC. SHALL BE PAINTED WHITE.
6. ALL BARRICADES SHALL BE CHECKED VISUALLY FOR SIGNS OF WEAR AND TEAR ON A WEEKLY BASIS AND SHALL BE REPAINTED WHEN DEEMED APPROPRIATE BY THE ENGINEER. THE CONDITIONS OF LIGHTING UNITS SHALL BE CHECKED DAILY. ALL LIGHT FIXTURES SHALL BE VERIFIED OPERATING BY THE CONTRACTOR ON A DAILY BASIS BEFORE THE CONTRACTOR CEASES OPERATION FOR THE DAY.
7. ALL BARRICADES SHALL BE MOVED AT LEAST ONCE EACH WEEK AND THE CONTRACTOR SHALL SWEEP THE DEBRIS WHICH HAS ACCUMULATED AND REMOVE FROM THE SITE. THE BARRICADES SHALL THEN BE REPLACED AT THE APPROPRIATE LOCATION.
8. THE CONTRACTOR SHALL INSTALL, MAINTAIN, AND REMOVE BARRICADES.
9. REFER TO SHEET G-04 FOR ADDITIONAL INFORMATION ON SAFETY AND SECURITY.

PHASING NOTES:

PHASE I:

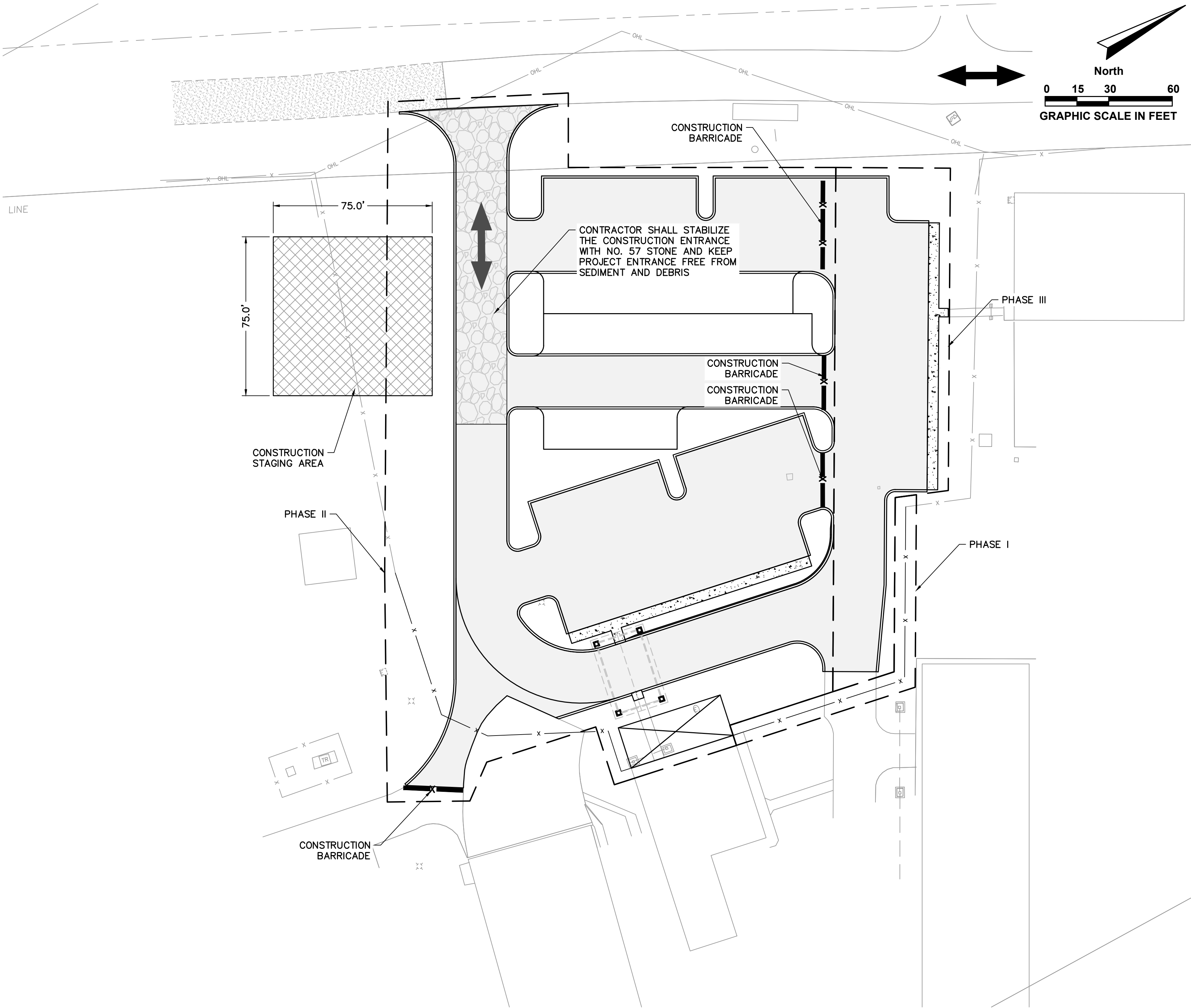
1. CONTRACTOR SHALL RELOCATE FENCE NORTH OF THE TERMINAL BUILDING AND CONSTRUCT STORMWATER RETENTION AREA.

PHASE II:

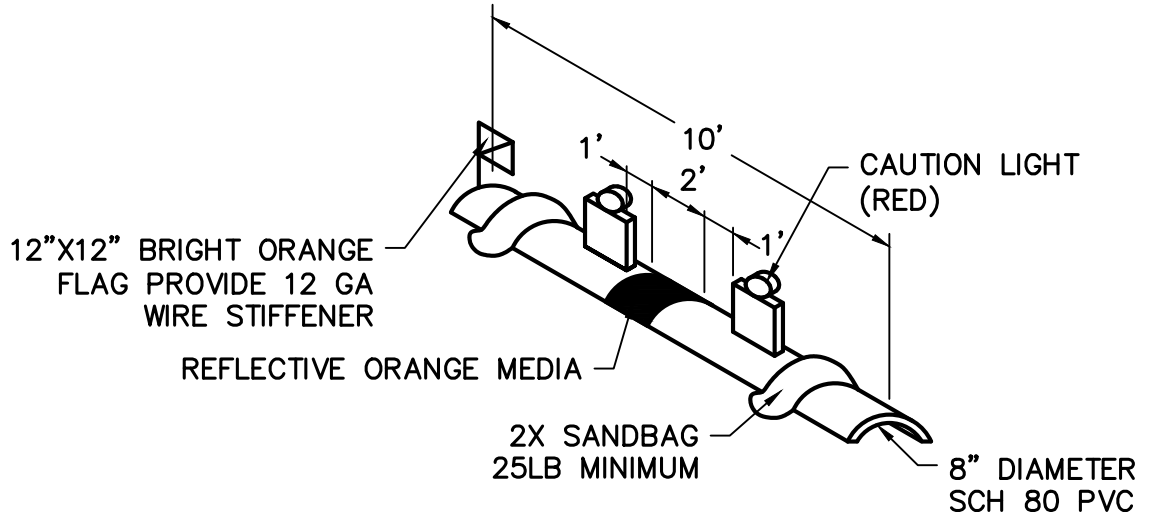
1. CONTRACTOR SHALL MAINTAIN EXISTING ACCESS DRIVE TO AIRFIELD.
2. CONTRACTOR MUST PROVIDE 72 HOUR NOTICE BEFORE CLOSING ACCESS TO EXISTING HANGAR.
3. DISRUPTION OF EXISTING HANGAR ACCESS SHALL NOT EXCEED 24 HOURS.

PHASE III:

1. CONTRACTOR SHALL PLACE BARRICADES ON NEW ASPHALT CONSTRUCTED IN PHASE II AS INDICATED BEFORE COMMENCING WITH PHASE III.



BENCHMARK INFORMATION			
BM#1:	N: 674027.213	BM#2:	N: 673666.024
	E: 1621981.763		E: 1622115.224
	ELEV: 89.35'		ELEV: 85.73'



CAUTION LIGHTS TO BE RED IN COLOR AND FLASHING DURING HOURS OF DARKNESS OR REDUCED VISIBILITY. UNITS TO BE PLACED ADJACENT TO EACH OTHER. ALTERNATE TYPES OF BARRICADES MAY BE APPROVED ON A CASE BY CASE BASIS. PAYMENT FOR BARRICADES SHALL BE INCIDENTAL TO PAY ITEM M-101-1.

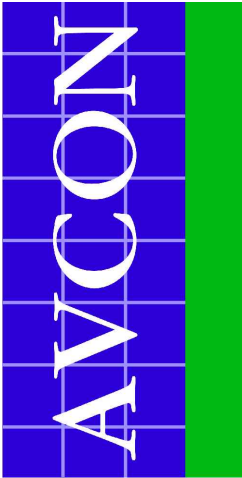
CONSTRUCTION BARRICADE DETAIL

N.T.S.

LEGEND			
TOFA	TAXIWAY/TAXILANE OBJECT FREE AREA	EXISTING PAVEMENT	
TSA	TAXIWAY/TAXILANE SAFETY AREA	STAGING AREA	
RUNWAY/TAXIWAY CENTERLINE		CONSTRUCTION ENTRANCE	
CONTRACTORS HAUL ROUTE		APPROXIMATE WETLAND LIMITS	

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SAFETY DURING  
CONSTRUCTION  
PLAN - PHASE 1

60% DESIGN DRAWINGS

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& SECURITY  
IMPROVEMENTS

PREPARED FOR

TRI-COUNTY AIRPORT AUTHORITY

DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
DATE: FEBRUARY, 2021

SHEET NUMBER

G-05

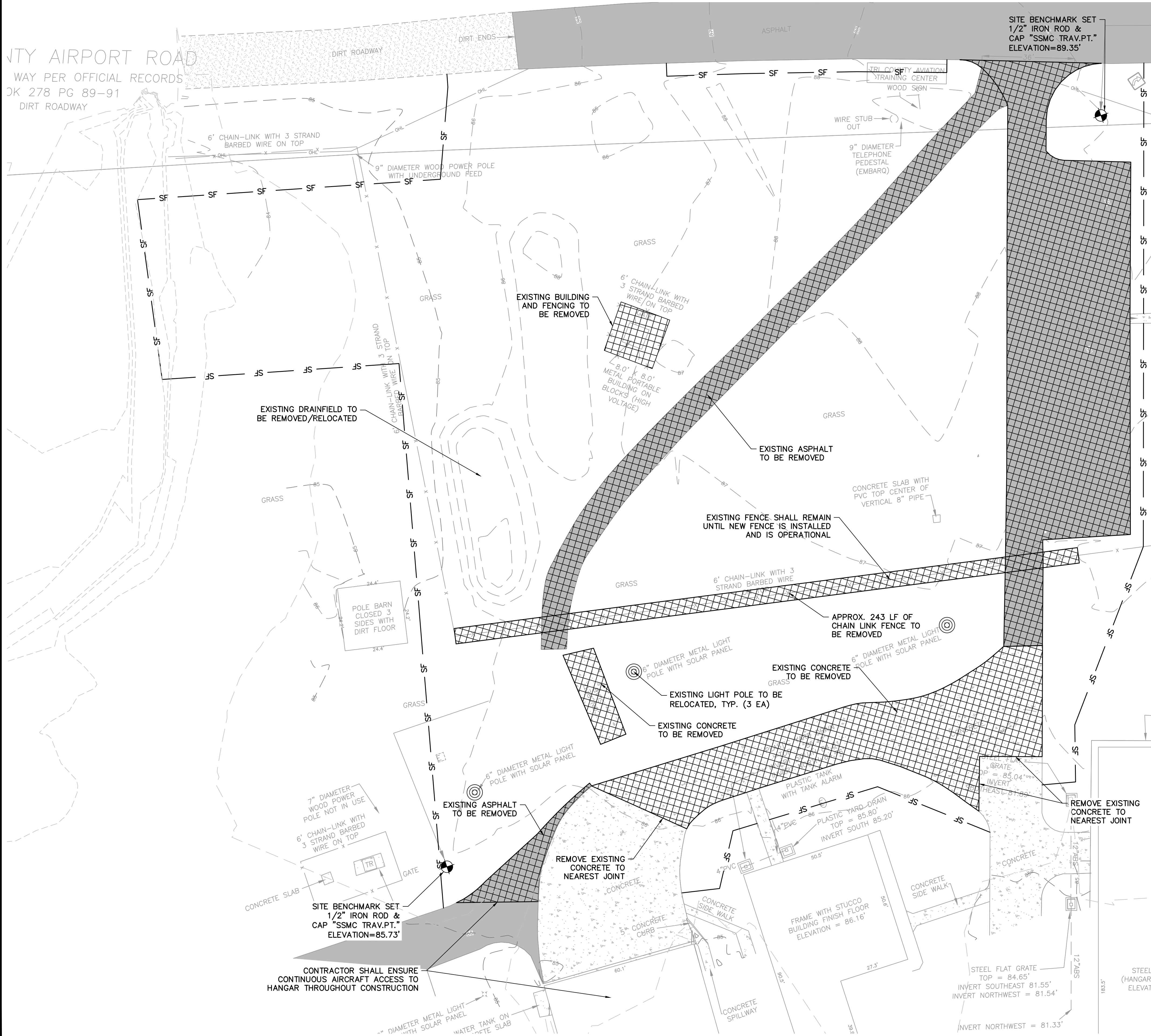
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ITY AIRPORT ROAD

WAY PER OFFICIAL RECORDS  
OK 278 PG 89-91  
DIRT ROADWAY

N:\NICEVILLE-PROJECTS\268--TRI-COUNTY AIRPORT AUTHORITY\2020.0268.01 TASK 09 TERMINAL ACCESS & SECURITY\000 CAD\20026801.DWG DEMO-EROSION.DWG 2/9/2021 3:34 PM



DEMOLITION LEGEND

- PROTECT ALL EXISTING ITEMS TO REMAIN DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL ENSURE THE INTEGRITY OF ALL ITEMS DENOTED TO BE PROTECTED THAT ARE ADJACENT TO ITEMS DENOTED TO BE DEMOLISHED AND WILL SAFELY REPAIR ANY SUCH ITEMS TO THE REQUIRED JURISDICTIONAL STANDARDS.
- SAWCUT AND REMOVE EXISTING PAVEMENT. SEE PAVEMENT DETAILS SHEET C-06.

EROSION NOTES

CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES IN ACCORDANCE TO FDOT STANDARDS.

CONTRACTOR SHALL REFER TO THE FOLLOWING FDOT DESIGN STANDARD DETAILS:

- FDOT 102, 103, 104, 105 AND 106.

CALL 2 WORKING DAYS BEFORE YOU DIG

811

IT'S THE LAW! DIAL 811

Know what's below. Call before you dig.

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

SEQUENCING OF ASPHALT PAVEMENT DEMOLITION

- CONTRACTOR SHALL SAW CUT AROUND THE FINAL FOOT PRINT TO THE FULL DEPTH OF THE ASPHALT PAVEMENT STRUCTURE.
- THE CONTRACTOR SHALL THEN OFFSET 1' (ONE FOOT) MINIMUM TO THE INTERIOR SIDES OF THE PROPOSED ASPHALT AND SAW CUT THE FULL DEPTH OF THE ASPHALT PAVEMENT.
- CONTRACTOR SHALL REMOVE ASPHALT PAVEMENT ON THE INTERIOR OF CUT 2, BUT LEAVE REMAINING 1' OF ASPHALT BETWEEN CUTS 1 AND 2 FOR PROTECTION OF REMAINING ASPHALT.
- CONTRACTOR MAY THEN REMOVE REMAINING ASPHALT STRIP BY BREAKING IT DOWN INTO SMALLER MORE MANAGEABLE SIZED PIECES FOR REMOVAL.
- REMOVE SUBGRADE MATERIAL SUFFICIENT TO PLACE REQUIRED AMOUNT OF BASE AND ASPHALT PER PLAN AND COMPACT SUBGRADE TO 100% OF THE MODIFIED PROCTOR VALUE AT OPTIMUM MOISTURE PER P-152.
- PLACE NEW P-209 BASE MATERIAL TO THICKNESS SPECIFIED IN PLANS AND COMPACT TO 100% OF THE MODIFIED PROCTOR VALUE AT OPTIMUM MOISTURE PER P-209.

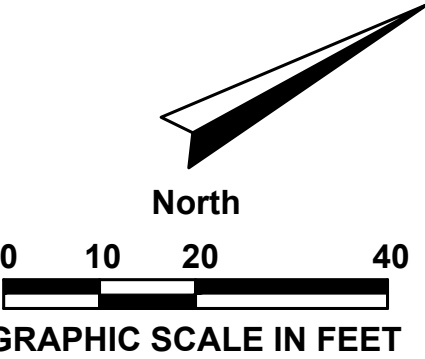
GENERAL SURVEY NOTES

- THE PURPOSE OF THIS SURVEY IS TO REFLECT TO THE BOUNDARY LIMITS AND TOPOGRAPHIC AREAS ARE PER SPECIFIC INSTRUCTIONS OF THE CLIENT.
- UTILITY LOCATIONS IF SHOWN HEREON ARE BASED ON FIELD LOCATION OF MARKINGS BY UTILITY COMPANY REPRESENTATIVES, SURFACE FEATURES AND CONSTRUCTION PLANS FURNISHED TO THE SURVEYOR. ADDITIONAL SUB-SURFACE UTILITIES MAY EXIST THAT HAVE NOT BEEN FIELD LOCATED.
- EASEMENTS OR RIGHTS OF WAY THAT APPEAR ON RECORDED PLANS OR THAT HAVE BEEN FURNISHED TO THE SURVEYOR BY OTHERS HAVE BEEN INCORPORATED INTO THIS DRAWING WITH APPROPRIATE NOTATION. OTHER EASEMENTS MAY BE DISCOVERED BY A SEARCH OF THE PUBLIC RECORDS.
- MINIMUM HORIZONTAL ACCURACY FOR THIS SURVEY IS IN ACCORDANCE WITH THE STANDARDS OF PRACTICE SET FORTH BY THE BOARD OF PROFESSIONAL SURVEYORS AND MAPPERS IN CHAPTER 5J-17 REQUIREMENTS OF FLORIDA ADMINISTRATION CODE. THE MAP AND MEASUREMENT METHODS USED FOR THIS SURVEY MEET OR EXCEED THIS REQUIREMENT. THE DIMENSIONS SHOWN HEREON ARE IN UNITED STATES STANDARD SURVEY FEET AND DECIMALS THEREOF.
- THIS SURVEY DOES NOT DETERMINE OWNERSHIP OF THE LANDS SHOWN HEREON.
- UNDERGROUND FOUNDATIONS HAVE NOT BEEN LOCATED.
- SURVEY MAP AND REPORT OR THE COPIES THEREOF ARE NOT VALID WITHOUT THE SIGNATURE AND THE ORIGINAL RAISED SEAL OF A FLORIDA LICENSED SURVEYOR AND MAPPER.
- FEATURES SHOWN BY SYMBOL AS INDICATED IN THE LEGEND ARE NOT TO SCALE.
- ADDITIONS OR DELETIONS TO SURVEY MAPS OR REPORTS BY OTHER THAN THE SIGNING PARTY OR PARTIES IS PROHIBITED WITHOUT WRITTEN CONSENT OF THE SIGNING PARTY OR PARTIES.
- VERTICAL INFORMATION SHOWN HEREON IS BASED ON 5/8" IRON ROD & CAP, STAMPED "1J0 A 1995", ELEVATION 83.00, NAVD88.

NOTE:  
SURVEY PROVIDED BY SOUTHEASTERN SURVEYING & MAPPING CORPORATION  
DATED 10-22-20.

BENCHMARK INFORMATION	
BM#1: N:1622449.262 E:673140.411 ELEV: 84.72	BM#2: N:1622355.689 E:673266.150 ELEV: 84.26

LEGEND	
	DEMO
	EXISTING PAVEMENT
	EXISTING FENCE TO REMAIN
	SILT FENCE
	LIGHT POLE TO BE RELOCATED



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FL LICENSE NO.: 50619  
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NO.	DATE	REVISION	BY

EXISTING CONDITIONS,  
DEMOLITION &  
EROSION CONTROL  
PLAN  
60% DESIGN DRAWINGS

TERMINAL ACCESS  
& SECURITY  
IMPROVEMENTS  
PREPARED FOR  
TRI-COUNTY AIRPORT AUTHORITY

DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
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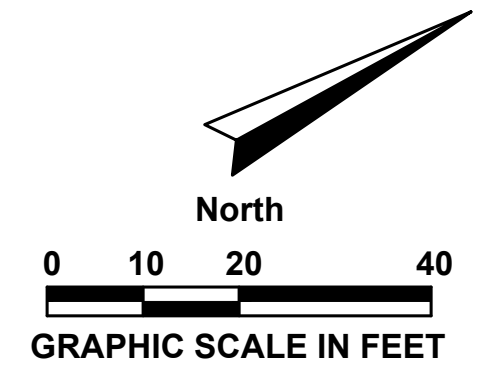
SHEET NUMBER  
C-01



EXISTING CONDITIONS

PROPOSED ASPHALT PAVEMENT

PROPOSED CONCRETE SIDEWALK

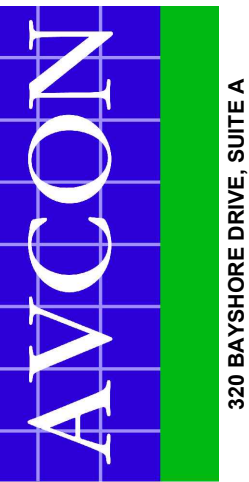


POINT TABLE		
POINT #	NORTHING	EASTING
1	673817.6840	1621848.5015
2	673885.0875	1621881.1918
3	673829.2565	1621883.2557
4	673852.0543	1621891.5901
5	673840.0973	1621913.4402
6	673842.0833	1621920.2267
7	673848.5135	1621923.7454
8	673855.2999	1621921.7595
9	673862.5006	1621908.6010
10	673925.6620	1621943.1645
11	673918.2213	1621956.7616
12	673920.0087	1621962.8694
13	673926.1165	1621961.0820
14	673933.5572	1621947.4845
15	674004.6138	1621986.3688
16	673996.9209	1622000.4269
17	673998.8778	1622007.1973
18	674011.8312	1622014.3975
19	673950.6128	1622124.5265
20	673930.7028	1622119.1794
21	673911.2340	1622154.2027
22	673888.2863	1622188.3329
23	673867.1282	1622176.6541

POINT TABLE		
POINT #	NORTHING	EASTING
24	673858.8373	1622156.4195
25	673745.2653	1622134.7455
26	673730.0728	1622114.6396
27	673691.1561	1622142.9364
28	673668.0312	1622128.9919
29	673712.6214	1622096.3948
30	673830.5621	1621941.2805
31	673951.5197	1622007.4716
32	673956.2476	1622023.8288
33	673945.3997	1622043.3433
34	673938.0999	1622045.4272
35	673811.3601	1621976.3707
36	673809.3742	1621969.5837
37	673823.7757	1621943.2664
38	673799.8390	1621997.4239
39	673922.1902	1622064.3776
40	673926.1300	1622078.0086
41	673922.9565	1622083.7177
42	673917.1866	1622086.1155
43	673913.2126	1622080.2629
44	673916.0244	1622065.5288
45	673854.1412	1622053.7190
46	673851.2356	1622068.9443

POINT TABLE		
POINT #	NORTHING	EASTING
47	673845.9718	1622072.5209
48	673842.3952	1622067.2571
49	673845.3007	1622052.0319
50	673783.4175	1622040.2222
51	673780.6057	1622054.9563
52	673774.7571	1622058.9304
53	673768.5768	1622057.7509
54	673765.1279	1622050.4393
55	673793.0525	1621999.4099
56	673758.4001	1622080.2419
57	673767.1310	1622081.9051
58	673772.4612	1622088.0187
59	673769.6545	1622102.7507
60	673886.5405	1622125.0571
61	673889.3524	1622110.3230
62	673895.2010	1622106.3489
63	673902.1279	1622107.6708
64	673905.5608	1622115.0115
65	673901.6819	1622121.9896
66	673874.0831	1622134.8963
67	673780.8026	1622117.0947
68	673752.4793	1622085.5597

1. ALL CURBING SHALL BE 1' RIBBON CURB UNLESS OTHERWISE STATED ON THE SITE PLAN.
2. ALL WORK DETAILED IN CONSTRUCTION DRAWINGS SHALL CONFORM TO FDOT STD. PLANS AND SPECIFICATIONS, LATEST EDITION.
3. CONTRACTOR SHALL MAINTAIN PUBLIC ACCESS AT ALL TIMES ALONG ALL PUBLIC RIGHT-OF-WAYS.
4. ALL PAVEMENT MARKINGS SHALL CONFORM TO FDOT STD. PLANS INDEX 711.
5. ALL SIGNS AND PLACEMENTS OF SIGNS SHALL CONFORM TO FDOT STD. INDEXES 11860 AND 17302.
6. REFER TO FDOT STANDARD PLAN INDEXES 102-600 AND 102-602 FOR PLACEMENT OF TRAFFIC CONTROL DEVICES.
7. ALL DISTURBED AREAS SHALL BE SODDED WITHIN TWO WEEKS AFTER FINAL GRADING IS COMPLETED.
8. PAVEMENT SUBGRADE SHALL HAVE ALL UNSUITABLE MATERIALS REMOVED TO A DEPTH OF TWO FEET BELOW TOP OF SUBGRADE, AND 2.5 FEET BEYOND EDGE OF PAVEMENT OR BACK OF CURB. BACKFILL WITH SUITABLE MATERIAL.
9. THE SUBGRADE SHALL BE PREPARED IN ACCORDANCE WITH SECTION 160 OF THE FLORIDA DEPARTMENT OF TRANSPORTATION SPECIFICATIONS.
10. THE BASE SHALL BE IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE FLORIDA DEPARTMENT OF TRANSPORTATION SPECIFICATIONS. ALLOWABLE BASE MATERIALS ARE INDICATED ON THE DRAWINGS.
11. ALL EXCAVATION AND EMBANKMENT CONSTRUCTION SHALL BE IN ACCORDANCE WITH SECTION 120 OF THE FLORIDA DEPARTMENT OF TRANSPORTATION SPECIFICATIONS.
12. WHERE DAMAGED OR REQUIRED TO BE CUT BY THE CONTRACTOR'S OPERATIONS, SIDEWALKS AND DRIVEWAYS SHALL BE REPAIRED TO CONFORM TO THE EXISTING TYPE OF CONSTRUCTION IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION SPECIFICATIONS.
13. PROVIDE CONTRACTION JOINTS AT 10' O.C. AND EXPANSION JOINTS AT 50' O.C. ON ALL EXTERIOR SIDEWALKS AND CURBING.
14. ALL PERMANENT PAINT STRIPING WITHIN THE RIGHT-OF-WAY SHALL BE THERMO-PLASTIC REFLECTIVE PAINT, AND BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
15. THERMO-PLASTIC PAINT SHALL BE INCIDENTAL TO THE PARKING LOT PAVEMENT MARKING PAY ITEM. NO SEPARATE PAYMENT SHALL BE MADE FOR THERMO-PLASTIC MARKINGS.
16. REFER TO SHEET C-07 FOR PARKING STRIPING DETAILS.
17. CONTRACTOR SHALL INSTALL WHEEL STOPS ON ALL PAVED PARKING SPACES. SEE SHEET C-07 FOR WHEEL STOP DETAILS.



**VIRGIL C. "LEE" LEWIS, P.E.**  
**FL LICENSE NO.: 50619**  
**FBPR CERTIFICATE OF**  
**AUTHORIZATION NO. 5057**

[illegible]

# SITE & GEOMETRY PLAN

## 60% DESIGN DRAWINGS

## TERMINAL ACCESS & SECURITY IMPROVEMENTS

TRI-COUNTY AIRPORT AUTHORITY

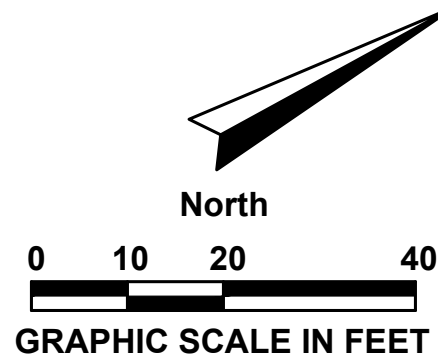
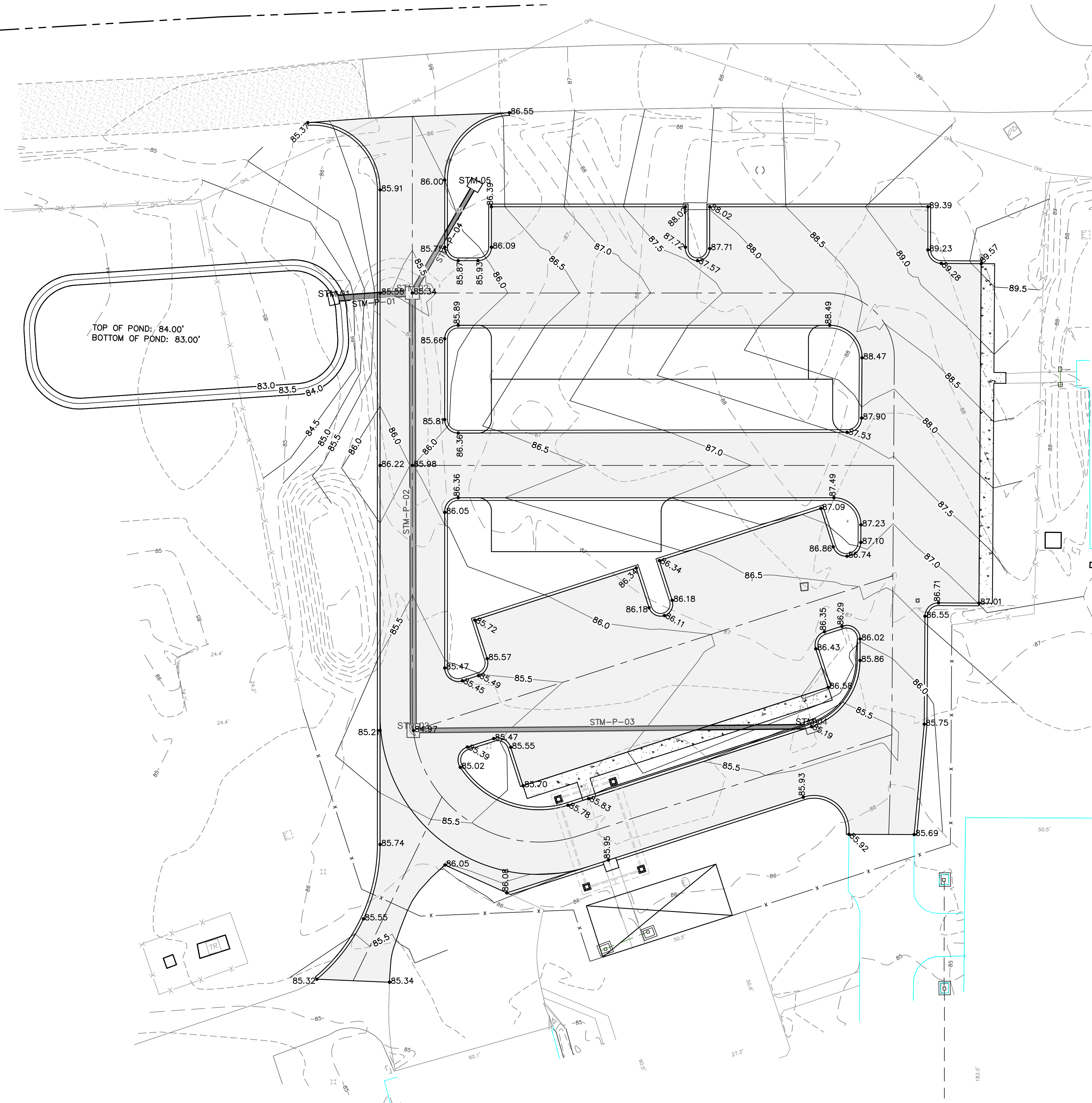
DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
DATE: FEBRUARY, 2021

SHEET NUMBER

# C-02



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GRADING AND DRAINAGE NOTES:

- SIDE SLOPES OF ALL SWALES AND PONDS SHALL BE NO STEEPER THAN 4:1. (MAX).
- CONTRACTOR SHALL INSTALL SILT FENCE PRIOR TO COMMENCING IN ANY CONSTRUCTION ACTIVITIES AND MAINTAIN SILT FENCE THROUGHOUT CONSTRUCTION.
- CONTRACTOR SHALL NOTIFY THE ENGINEER IF EROSION CONTROL ISSUES DEVELOP ONSITE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR EROSION CONTROL MEASURES ON SITE 24 HOURS A DAY THROUGHOUT CONSTRUCTION.
- CONTRACTOR SHALL PROVIDE AND MAINTAIN INLET PROTECTION THROUGHOUT CONSTRUCTION SEE SHEET C-07 FOR DETAIL.
- ALL DISTURBED AREAS (INCLUDING RE-GRADED AREA) SHALL BE RETURNED TO ORIGINAL CONDITION.
- ALL SUITABLE MATERIAL EXCAVATED SHALL BE USED IN THE FORMATION OF EMBANKMENTS OR PLACED IN STOCKPILES FOR FUTURE USE IN ACCORDANCE WITH THE PLANS AND AS DIRECTED BY THE ENGINEER. UNSUITABLE MATERIAL EXCAVATED SHALL BE DISPOSED OF BY THE CONTRACTOR AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- UNSUITABLE OR UNSTABLE MATERIALS ENCOUNTERED IN THE SUBGRADE PREPARATION SHALL BE UNDERCUT AND STABILIZED AS SPECIFIED IN THE PROJECT SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL ALWAYS OBTAIN ENGINEER AUTHORIZATION PRIOR TO BEGINNING ANY UNDERCUT EXCAVATION OPERATIONS. UPON COMPLETION OF THE UNDERCUT AND ACCEPTANCE OF THE RESULTING STABLE SUBGRADE BY THE ENGINEER, THE CONTRACTOR SHALL SURVEY THE BOTTOM OF THE UNDERCUT AREA FOR THE PURPOSES OF ESTABLISHING PAYMENT QUANTITIES.
- EXCAVATION LIKELY TO DISLOCATE, DAMAGE, OR IMPAIR THE STRENGTH OF OTHER STRUCTURES ALREADY IN PLACE SHALL BE DONE ONLY AFTER ADEQUATE PROTECTION HAS BEEN PROVIDED FOR THE IN-PLACE STRUCTURES.
- CONTRACTOR SHALL GRADE ALL AREAS AS INDICATED OR DIRECTED BY OWNER. FILL SHALL BE BROUGHT TO FINISH GRADES AS SHOWN AND SHALL BE GRADED TO DRAIN WATER AWAY FROM STRUCTURES.
- ALL LENGTHS OF PIPES SHOWN ARE SCALED ONLY. DRAINAGE, ENDWALLS, AND PIPES SHALL BE CONSTRUCTED TO CONFORM WITH THE LOW POINTS AND OTHER PERTINENT FEATURES.
- CONTRACTOR SHALL PROMOTE POSITIVE DRAINAGE TOWARDS INLETS, NO "BIRD BATHS" OR PONDING SHALL BE ACCEPTED.
- UNPAVED AREAS SHALL BE GRADED TO PROMOTE POSITIVE DRAINAGE TO THE STORMWATER DETENTION AREA OR ASSOCIATED INLET.

STORMWATER STRUCTURE TABLE			
STRUCTURE NAME	STRUCTURE TYPE	DETAILS	NORTHING/EASTING
STM-01	FDOT TYPE 'C' DBI	RIM = 83.25 STM-P-01 INV IN = 77.75	N: 673794.70 E: 1621910.3405
STM-02	FDOT TYPE 'G' DBI	RIM = 85.34 STM-P-02 INV IN = 79.00 STM-P-04 INV IN = 79.41 STM-P-01 INV OUT = 77.82	N: 673821.40 E: 1621922.5864
STM-03	FDOT TYPE 'F' DBI	RIM = 84.97 STM-P-03 INV IN = 79.41 STM-P-02 INV OUT = 79.41	N: 673743.74 E: 1622065.2273
STM-04	FDOT TYPE 4 CURB INLET	RIM = 85.19 STM-P-03 INV OUT = 79.92	N: 673874.08 E: 1622134.8963
STM-05	FDOT TYPE 'C' DBI	RIM = 86.25 STM-P-04 INV OUT = 79.57	N: 673860.94 E: 1621898.6188

STORMWATER PIPE TABLE			
NAME	DESCRIPTION	LENGTH	SLOPE
STM-P-01	24" ADS, N-12	29.37'	0.25%
STM-P-02	24" ADS, N-12	162.41'	0.25%
STM-P-03	18" ADS, N-12	147.80'	0.35%
STM-P-04	18" ADS, N-12	46.24'	0.35%

LEGEND	
	PROPOSED CONTOURS
	EXISTING CONTOURS
	DRAINAGE FLOW ARROW
	PROPOSED SPOT ELEVATION
	BID SCHEDULE A: ASPHALT PAVEMENT
	BID SCHEDULE B: ASPHALT PAVEMENT
	TSA TAXIWAY SAFETY AREA (TSA)
	TOFA TAXIWAY OBJECT FREE AREA (TOFA)

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TRI-COUNTY AIRPORT AUTHORITY  
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320 BAYSHORE DRIVE, SUITE A  
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FL LICENSE NO.: 50619  
FBPR CERTIFICATE OF AUTHORIZATION NO. 5057

NO.	DATE	REVISION	BY

GRADING & DRAINAGE PLAN

60% DESIGN DRAWINGS

TERMINAL ACCESS & SECURITY IMPROVEMENTS

PREPARED FOR  
TRI-COUNTY AIRPORT AUTHORITY

DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
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SHEET NUMBER  
C-03

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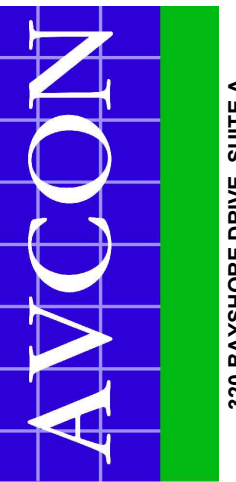
1. SOD SHALL BE CERTIFIED TO BE FREE OF MILLET SEED AND NOXIOUS WEEDS BY THE FLORIDA DEPARTMENT OF AGRICULTURE, DIVISION OF PLANT INDUSTRY.
2. ALL DISTURBED AREAS SHALL BE SODDED. IF AREAS TO BE SODDED ARE OUTSIDE OF THE LIMITS SHOWN ON THIS SHEET THE SODDING SHALL BE AT THE CONTRACTOR'S EXPENSE.
3. SOD FOR RESTORATION PURPOSES TO BE CENTIPEDE.
4. CONTRACTOR IS RESPONSIBLE FOR MINIMIZING AND REPAIRING EROSION DURING CONSTRUCTION AND SHALL BE RESPONSIBLE FOR PROTECTING NEW SOD FROM EROSION UNTIL THE SOD IS FULLY ESTABLISHED.
5. CONTRACTOR SHALL WATER SOD REGULARLY TO ENSURE SOD BECOMES ESTABLISHED. PAYMENT FOR SOD SHALL NOT BE PROVIDED UNTIL A HEALTHY STAND OF SOD IS ESTABLISHED.
6. ALL SOD IN AREAS WHICH EXCEED 6:1 SLOPES TO BE SECURED WITH 1" WOODEN PINS NOT LESS THAN 12" IN LENGTH DRIVEN FLUSH WITH THE SOD TO PREVENT SLIPPING OR DISPLACEMENT OF SOD.

## LEGEND

- 
- PROPOSED ASPHALT PAVEMENT
- LIMITS OF SOD
- LIMITS OF PYRAMAT WITH SOD

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AUTHORITY  
P.O. BOX 756  
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[illegible]

## SOD PLAN

## 60% DESIGN DRAWINGS

## TERMINAL ACCESS & SECURITY IMPROVEMENTS

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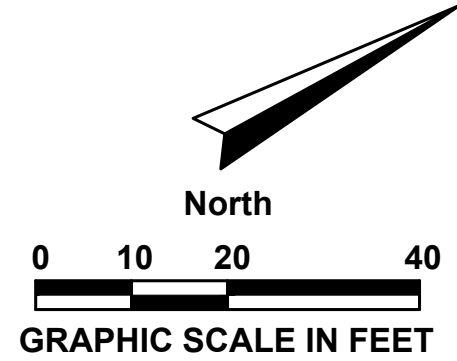
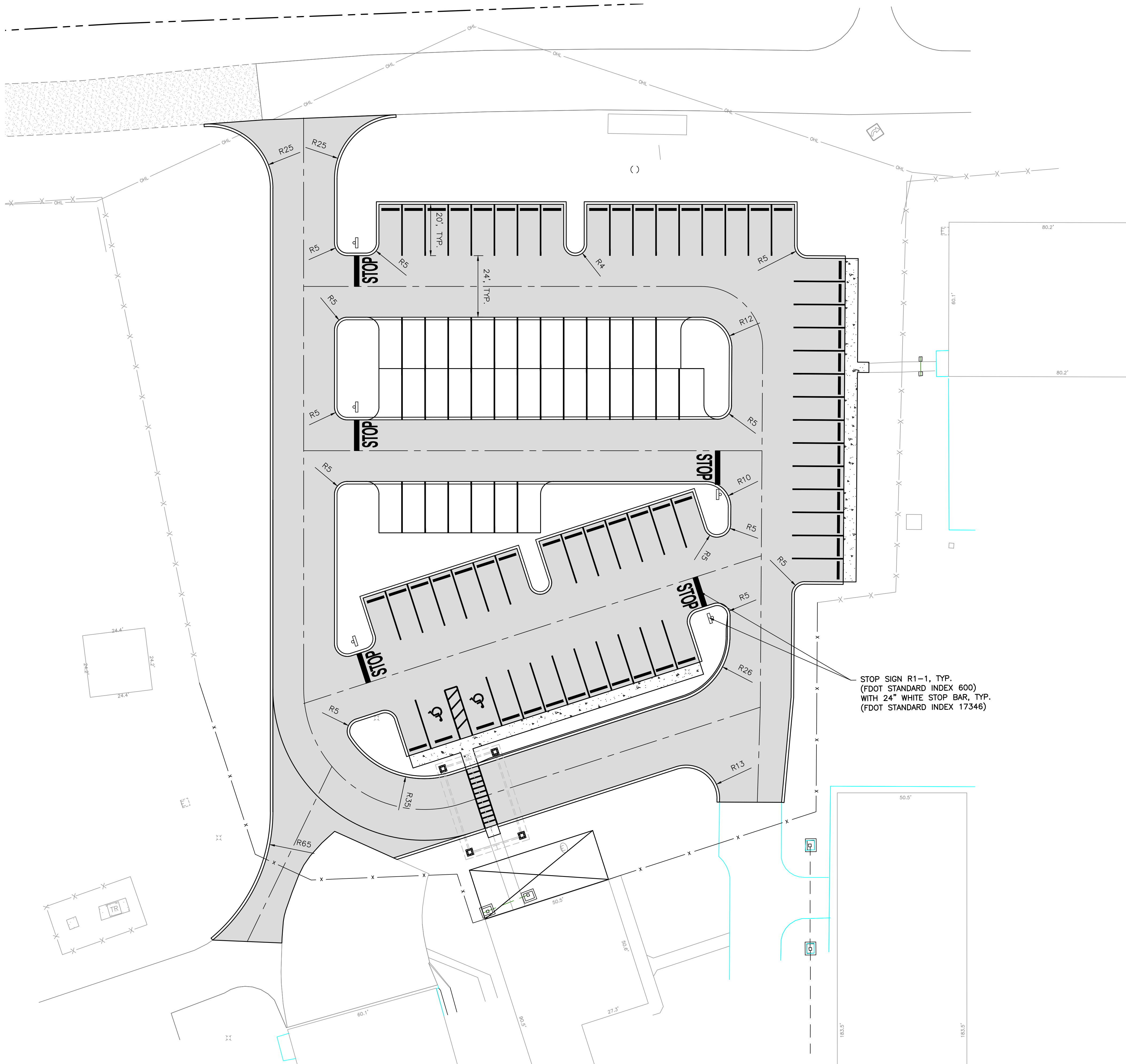
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DRAWN BY: M.B.  
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APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
DATE: FEBRUARY, 2021

SHEET NUMBER

**C-04**



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**NOTES:**

1. SEE SHEET C-10 FOR MARKING DETAILS.

**LEGEND**

PROPOSED MARKINGS

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PAVEMENT, MARKING  
& SIGNAGE PLAN  
60% DESIGN DRAWINGS

VIRGIL C. "LEE" LEWIS, P.E.  
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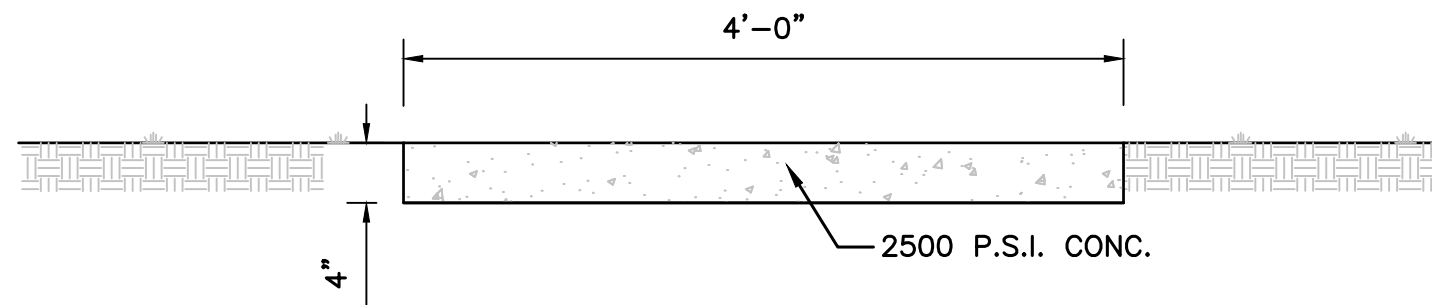
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C-05

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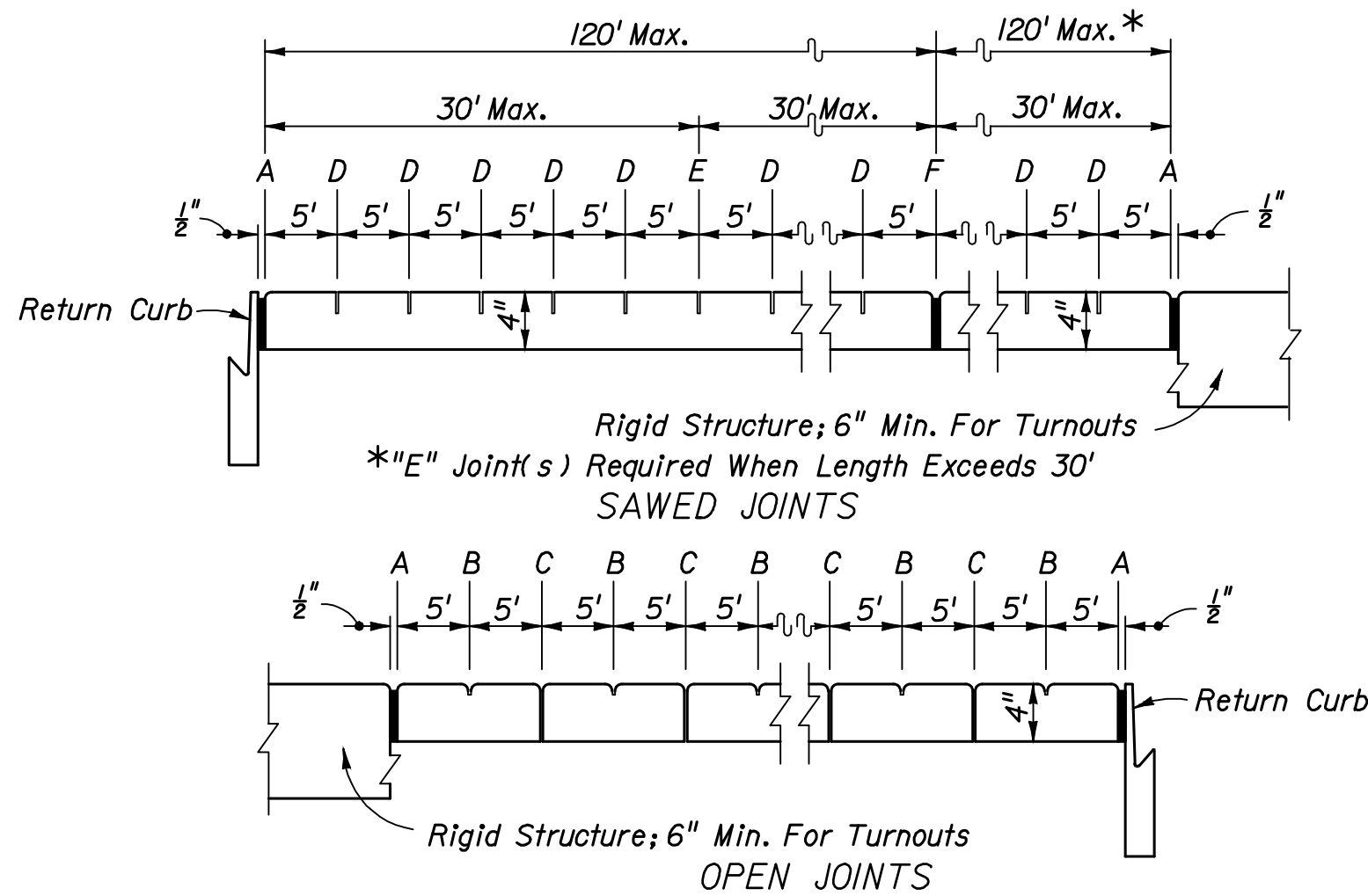


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### CONCRETE SIDEWALK DETAIL

FDOT STANDARD PLANS 502-301



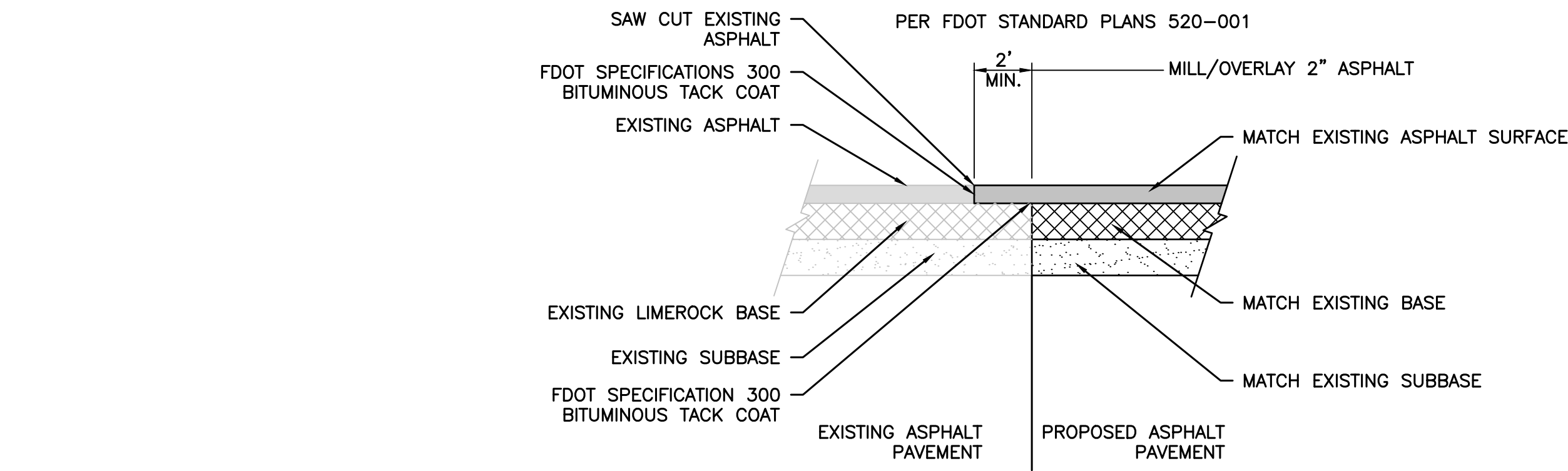
EXAGGERATED SCALE  
LONGITUDINAL SECTION  
SIDEWALK JOINTS

### 4' CONCRETE SIDEWALK JOINT DETAIL

N.T.S.

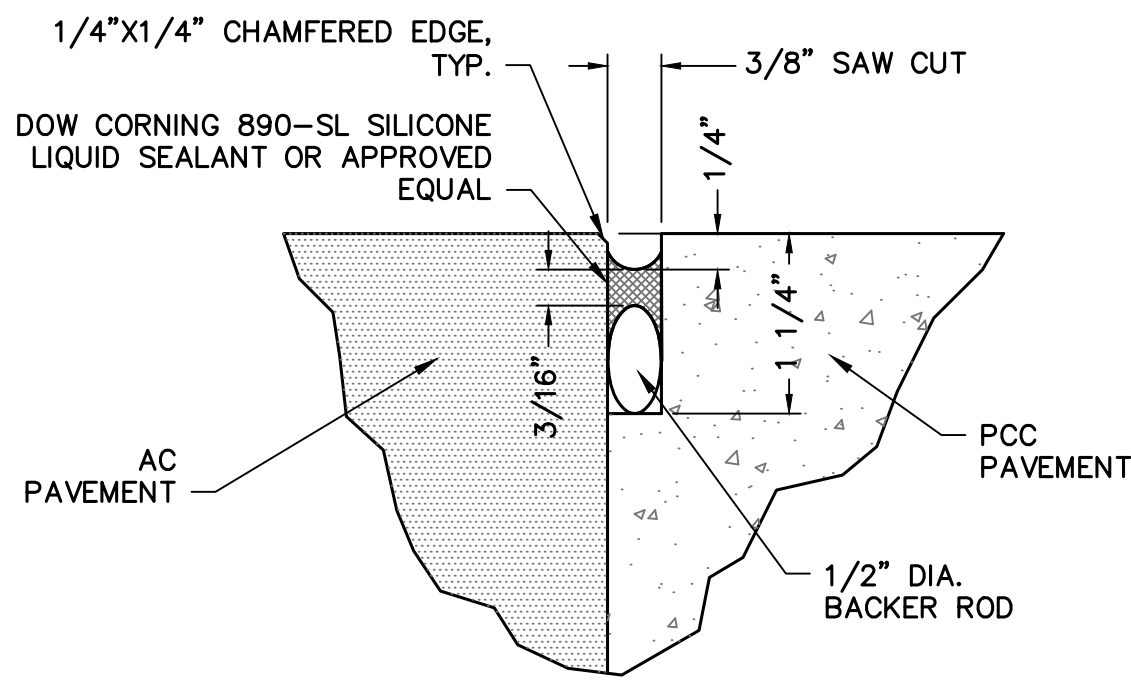
#### JOINT LEGEND

- A -  $\frac{1}{2}$ " Expansion Joints (Preformed Joint Filler)
- B -  $\frac{1}{8}$ " Dummy Joints, Tooled
- C -  $\frac{1}{8}$ " Formed Open Joints
- D -  $\frac{3}{16}$ " Saw Cut Joints,  $1\frac{1}{2}$ " Deep (96 Hour) Max. 5' Centers
- E -  $\frac{3}{16}$ " Saw Cut Joints,  $1\frac{1}{2}$ " Deep (12 Hour) Max. 30' Centers
- F -  $\frac{1}{2}$ " Expansion Joint When Run Of Sidewalk Exceeds 120'. Intermediate locations when called for in the plans or at locations as directed by the Engineer.
- G - Cold Joint With Bond Breaker, Tooled



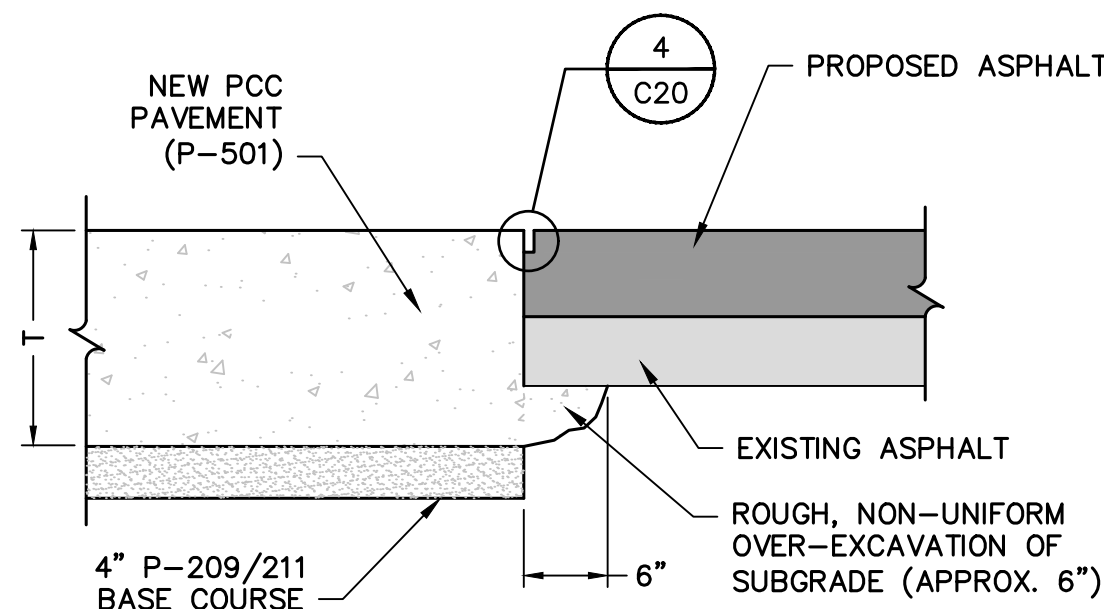
### ASPHALT/ASPHALT PAVEMENT BUTT JOINT DETAIL

N.T.S.



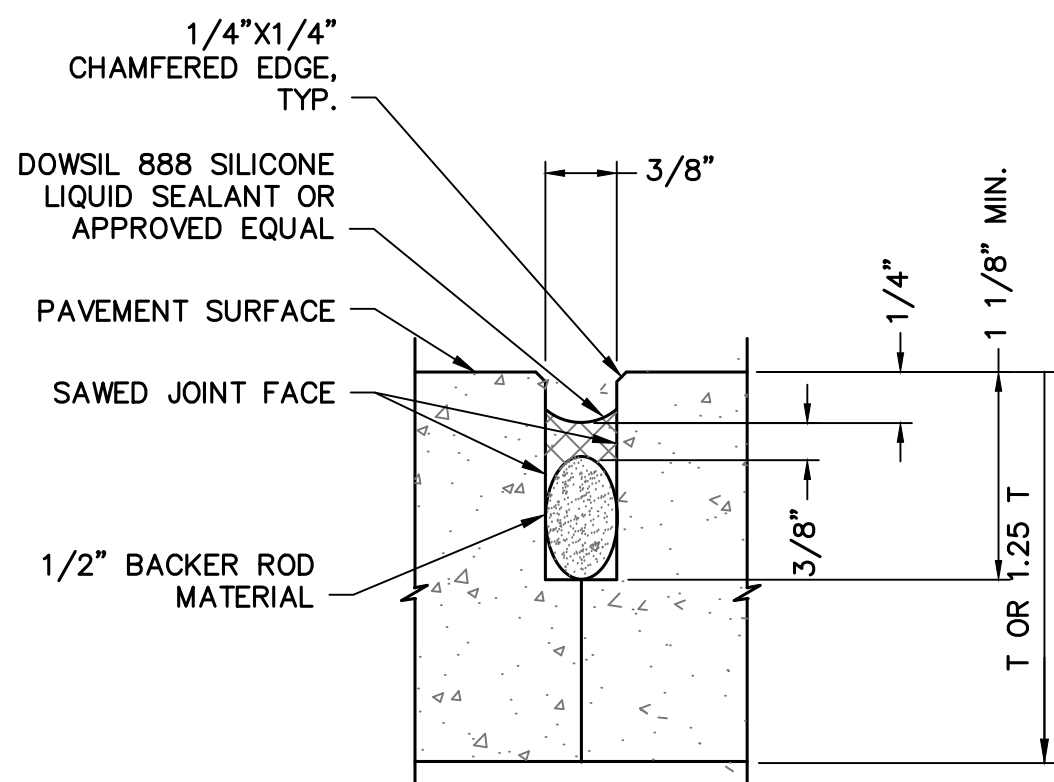
### PCC-AC JOINT SEAL

N.T.S.



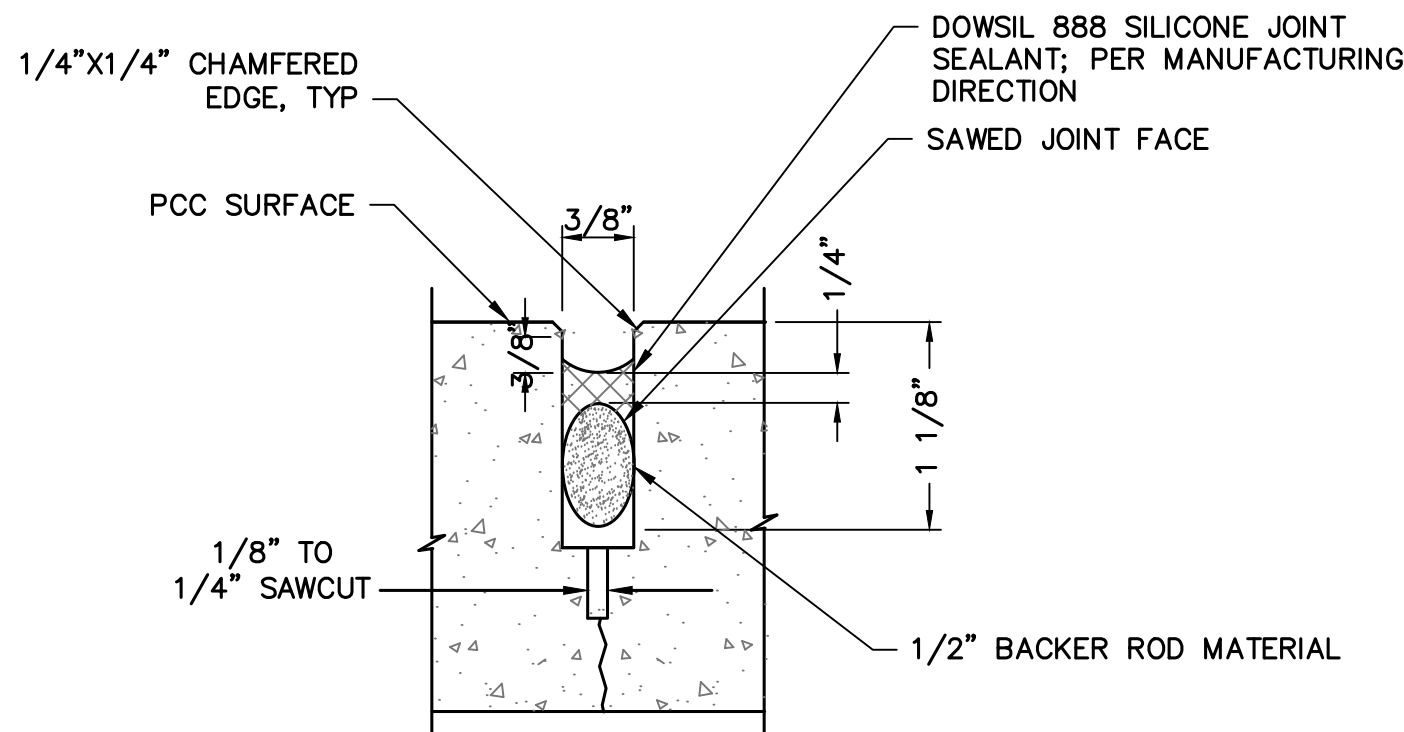
### TYPE R PCC-AC JOINT DETAIL

N.T.S.



### PCC CONSTRUCTION JOINT

N.T.S.

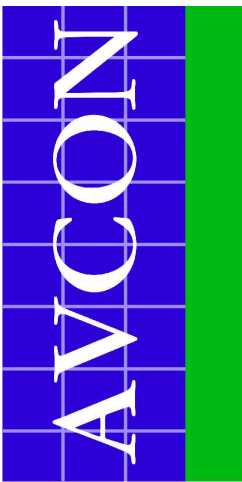


### PCC CONTRACTION JOINT DETAIL

N.T.S.

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MISCELLANEOUS  
DETAILS  
(SHEET 1 OF 2)

60% DESIGN DRAWINGS

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IMPROVEMENTS

PREPARED FOR

TRI-COUNTY AIRPORT AUTHORITY

DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
DATE: FEBRUARY, 2021

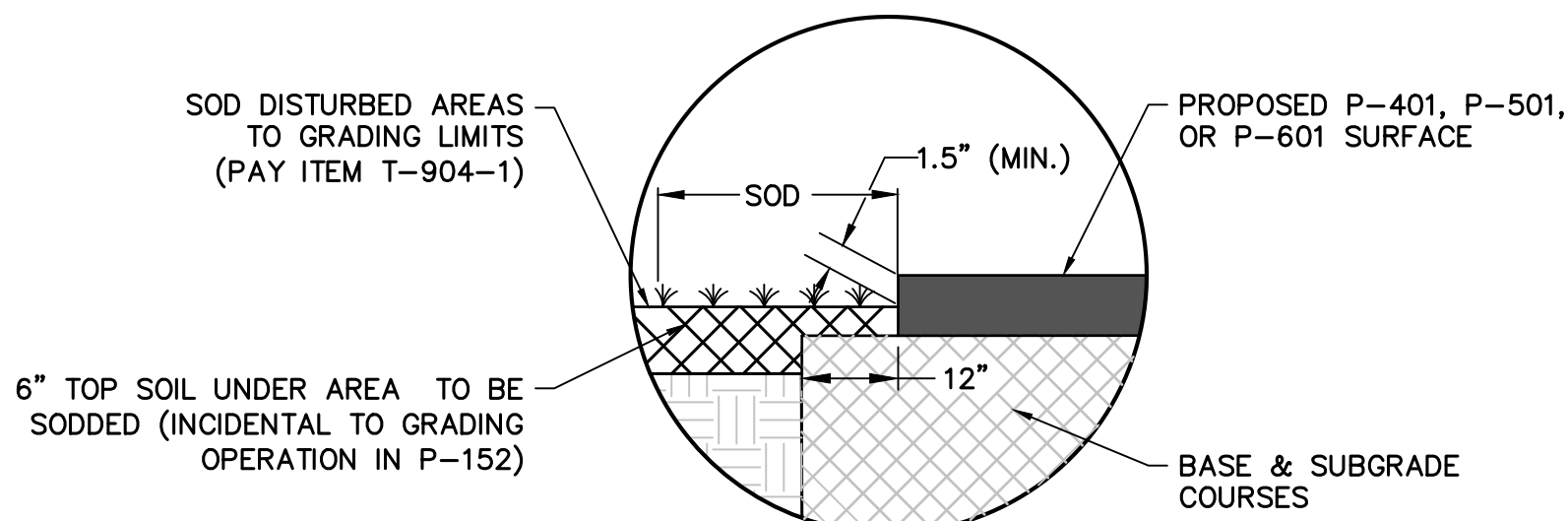
SHEET NUMBER

C-06

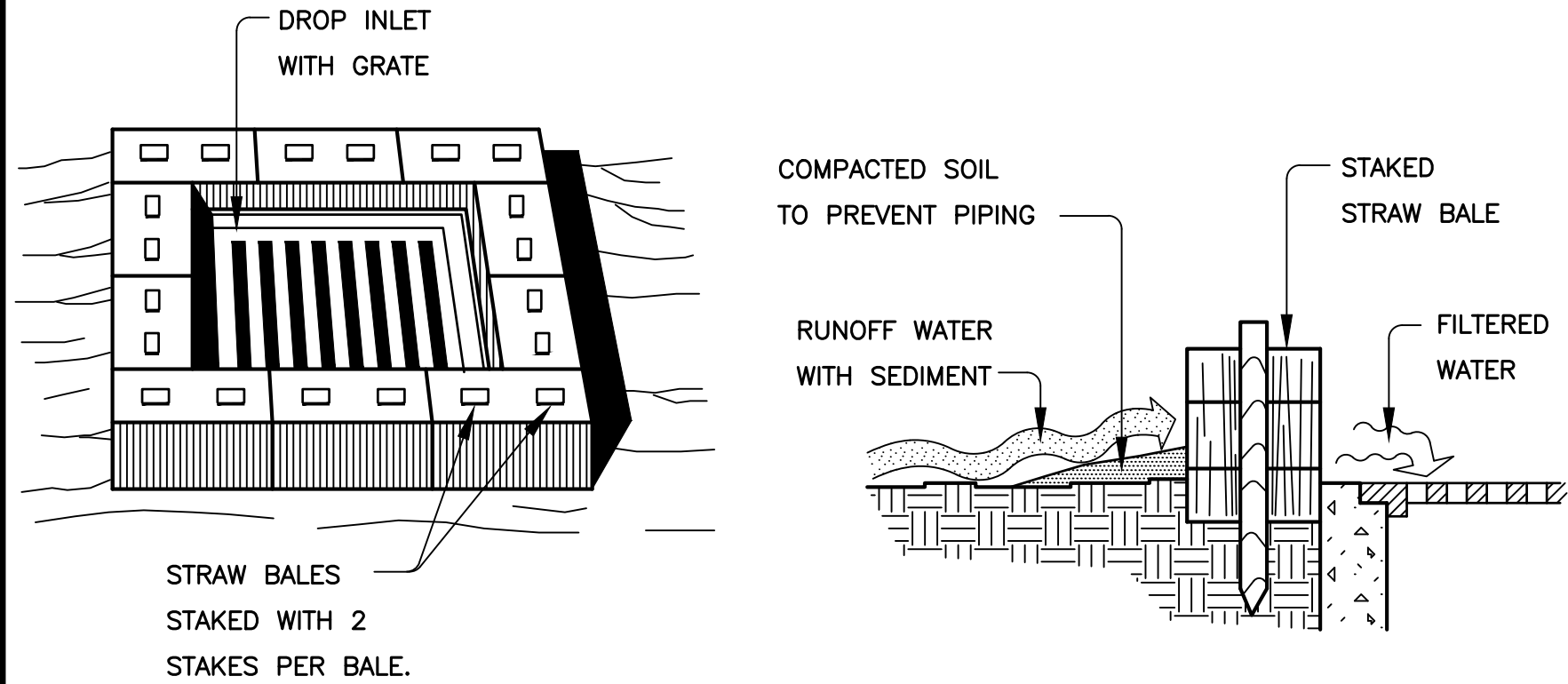
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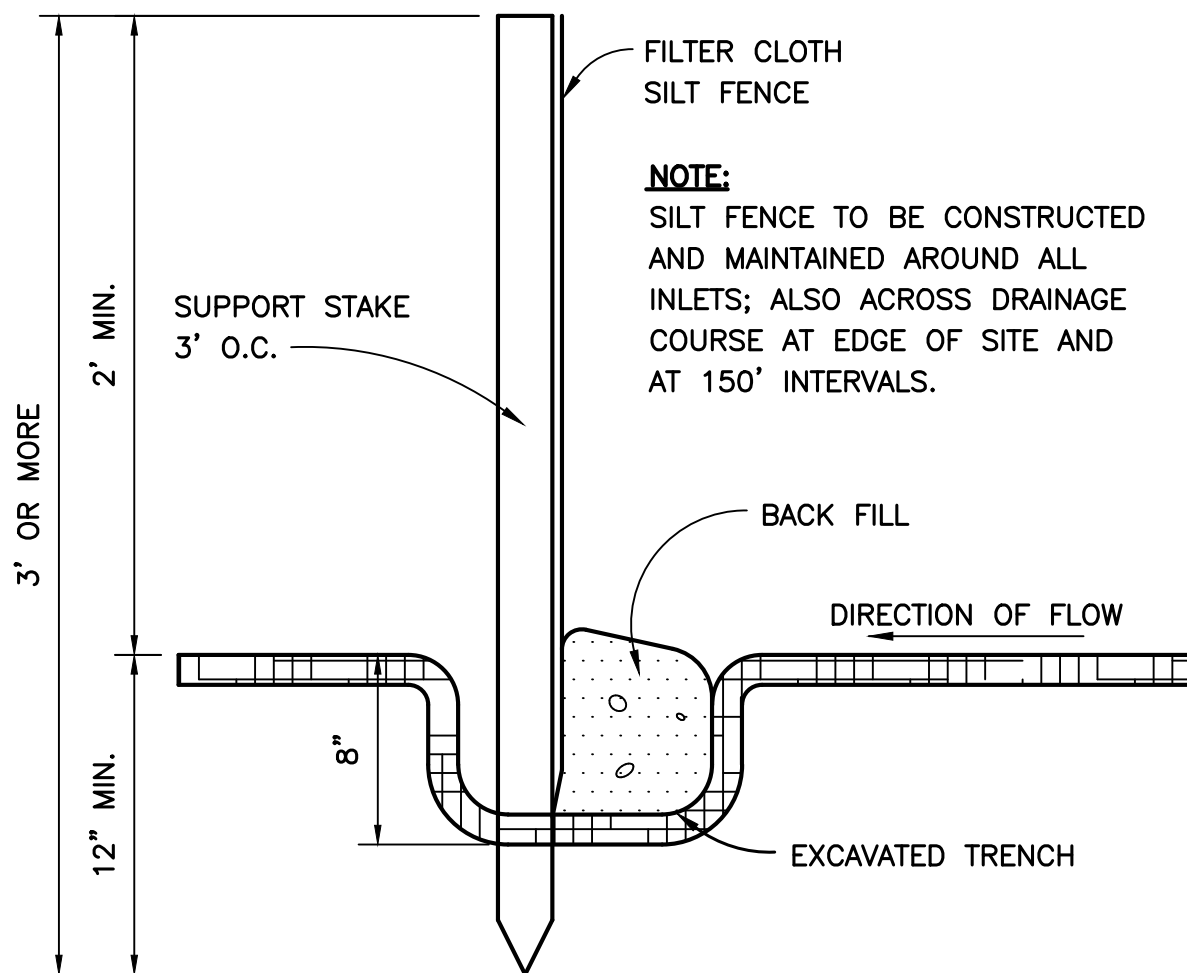
**DETAIL A - PAVEMENT DROP-OFF**  
N.T.S.



**STRAW BALE DROP INLET SEDIMENT FILTER**  
N.T.S.

**SPECIFIC APPLICATION**

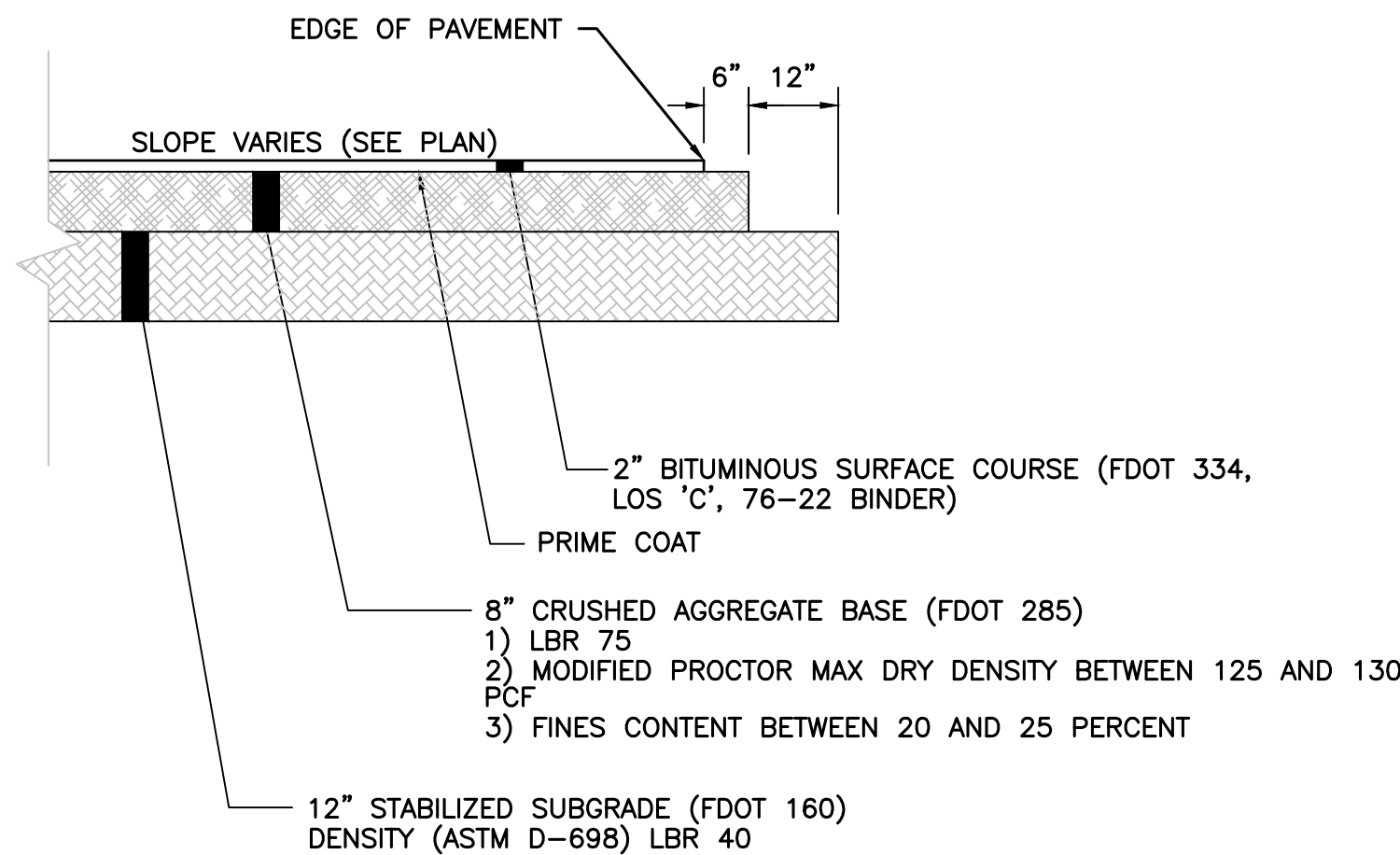
THIS METHOD OF INLET PROTECTION IS APPLICABLE WHERE THE INLET DRAINS A RELATIVELY FLAT AREA (SLOPES NO GREATER THAN 5 PERCENT) WHERE SHEET OR OVERLAND FLOWS (NOT EXCEEDING 0.5 CFS) ARE TYPICAL. THE METHOD SHALL NOT APPLY TO INLETS RECEIVING CONCENTRATED FLOWS, SUCH AS IN STREET OR HIGHWAY MEDIANS.



**SILT FENCE DETAIL**  
N.T.S.

**EROSION NOTES:**

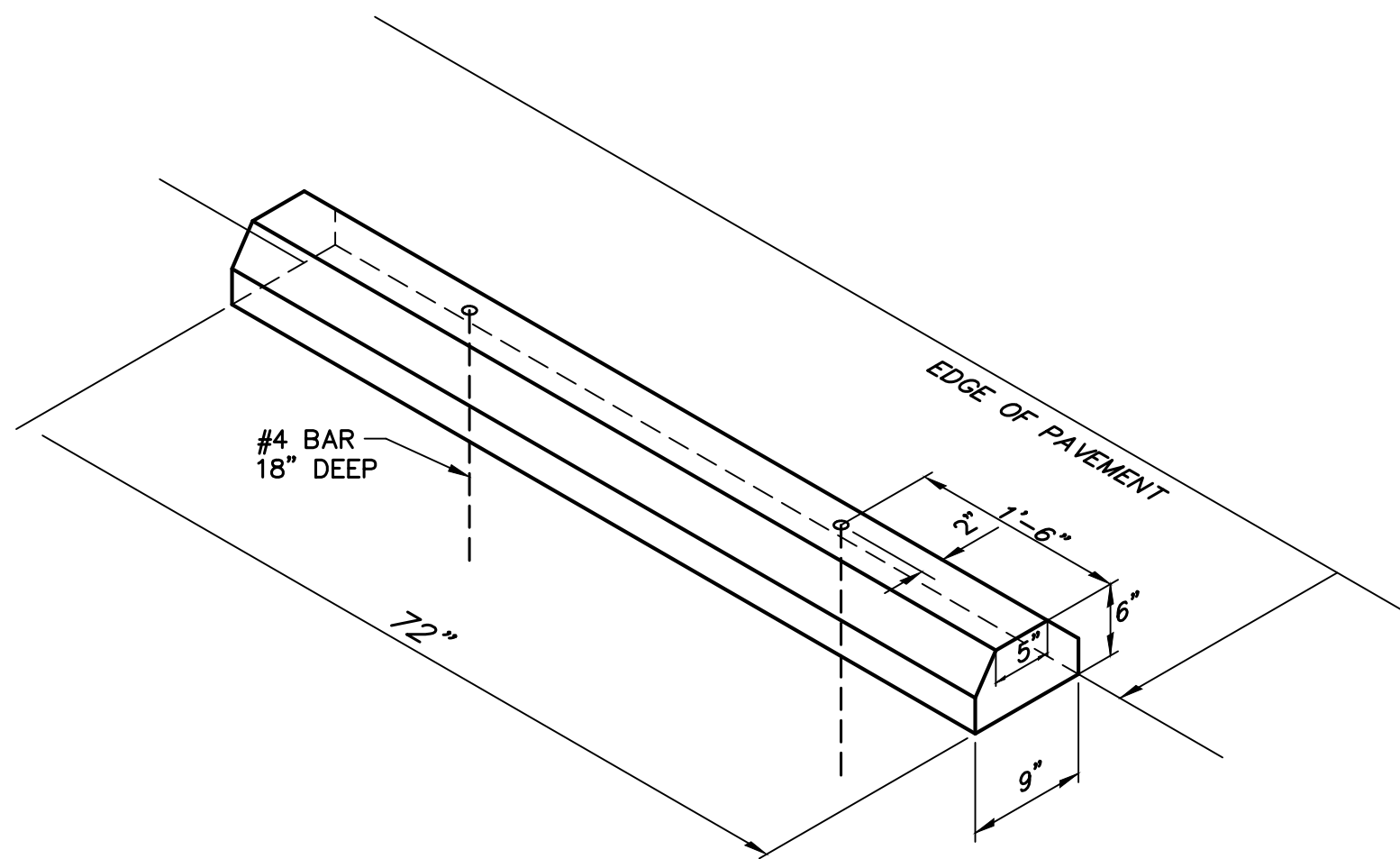
- ALL SILT FENCING SHALL BE INSTALLED AND SPACED ACCORDING TO FDOT STD. SPECIFICATIONS 104-4.
- EROSION PROTECTION, SUCH AS STAKED BALED HAY AND SILT FENCE BARRIERS, MUST BE INSTALLED PRIOR TO START OF CONSTRUCTION.
- SILT FENCE BARRIER SHALL BE INSTALLED AS SHOWN ON PLANS, AND IN ALL AREAS SUBJECT TO SOIL EROSION SEDIMENTATION, SPECIFICALLY ADJACENT TO ALL BODIES OF WATER AND WETLAND AREAS WHERE THERE IS A POTENTIAL FOR DOWNSTREAM WATER QUALITY DEGRADATION.
- SEDIMENT AND EROSION CONTROL DEVICES SHALL REMAIN IN PLACE THROUGHOUT CONSTRUCTION AND SHALL BE REMOVED AT COMPLETION OF THE PROJECT.



**TYPICAL PAVEMENT SECTION - PARKING LOT**  
N.T.S.

**NOTE:**

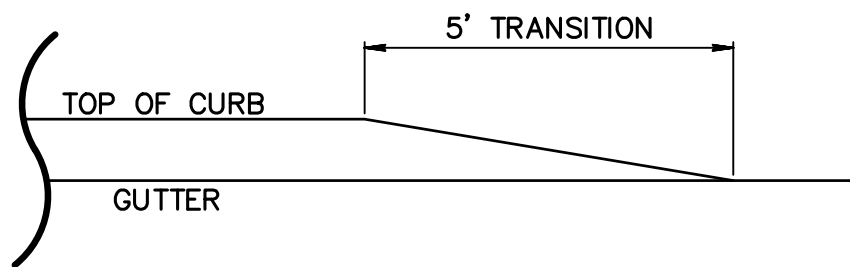
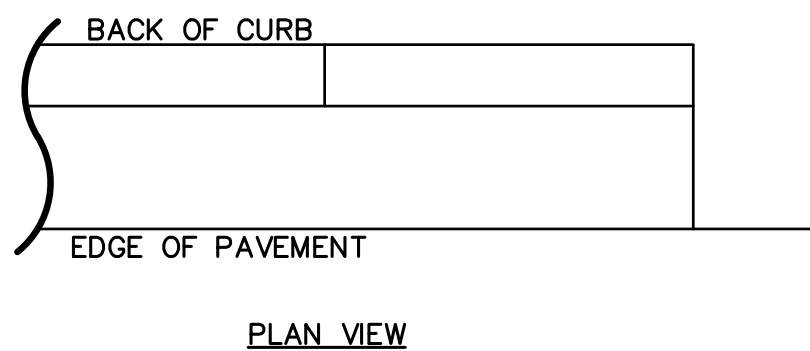
PRIME AND TACK COATS SHALL BE INCIDENTAL TO PAY ITEM 334-1. NO SEPARATE PAYMENT SHALL BE MADE FOR PRIME OR TACK COATS.



**NOTE:**

- WHEEL STOP SHALL BE BARCO PRODUCTS, MODEL # WHLSTP, YELLOW, 6" SOLID WHEEL STOP, 72"x6"x4", OR APPROVED EQUAL.
- ALL PROPOSED PARKING SPACES REQUIRE WHEEL STOPS.

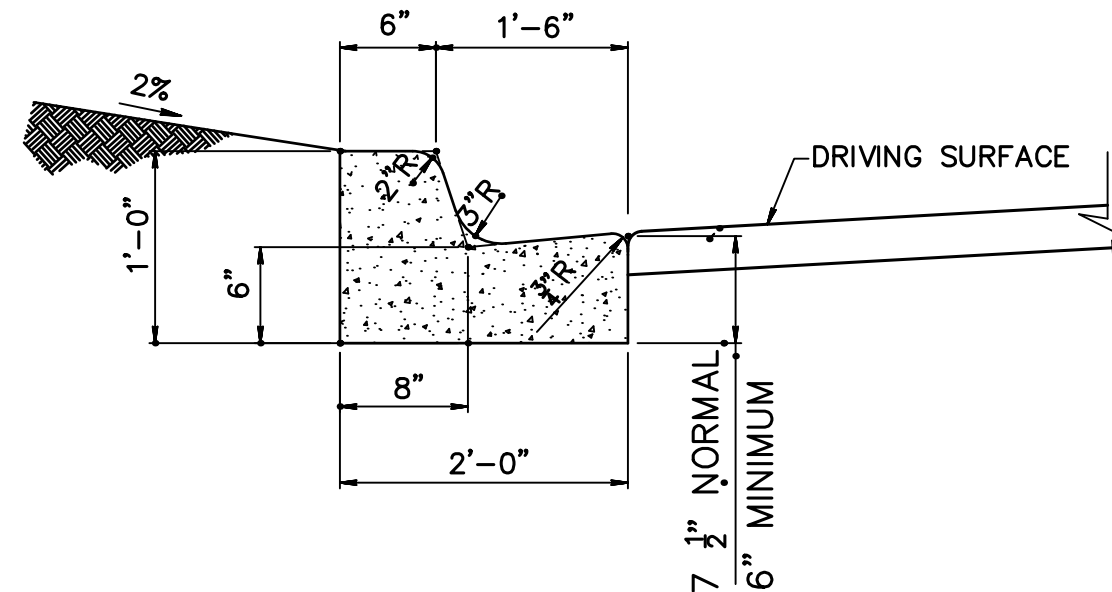
**WHEEL STOP (RECYCLABLE MATERIALS)**  
N.T.S.



**CURB TAPER DETAIL**  
N.T.S.

	STABILIZED SUBGRADE	12"
8"	100% MAX. DRY DENSITY (ASTM D-698)	12"
10"	95% MAX. DRY DENSITY (ASTM D-698)	12"
14"	90% MAX. DRY DENSITY (ASTM D-698)	12"
12"	85% MAX. DRY DENSITY (ASTM D-698)	
	NATURAL DENSITY	

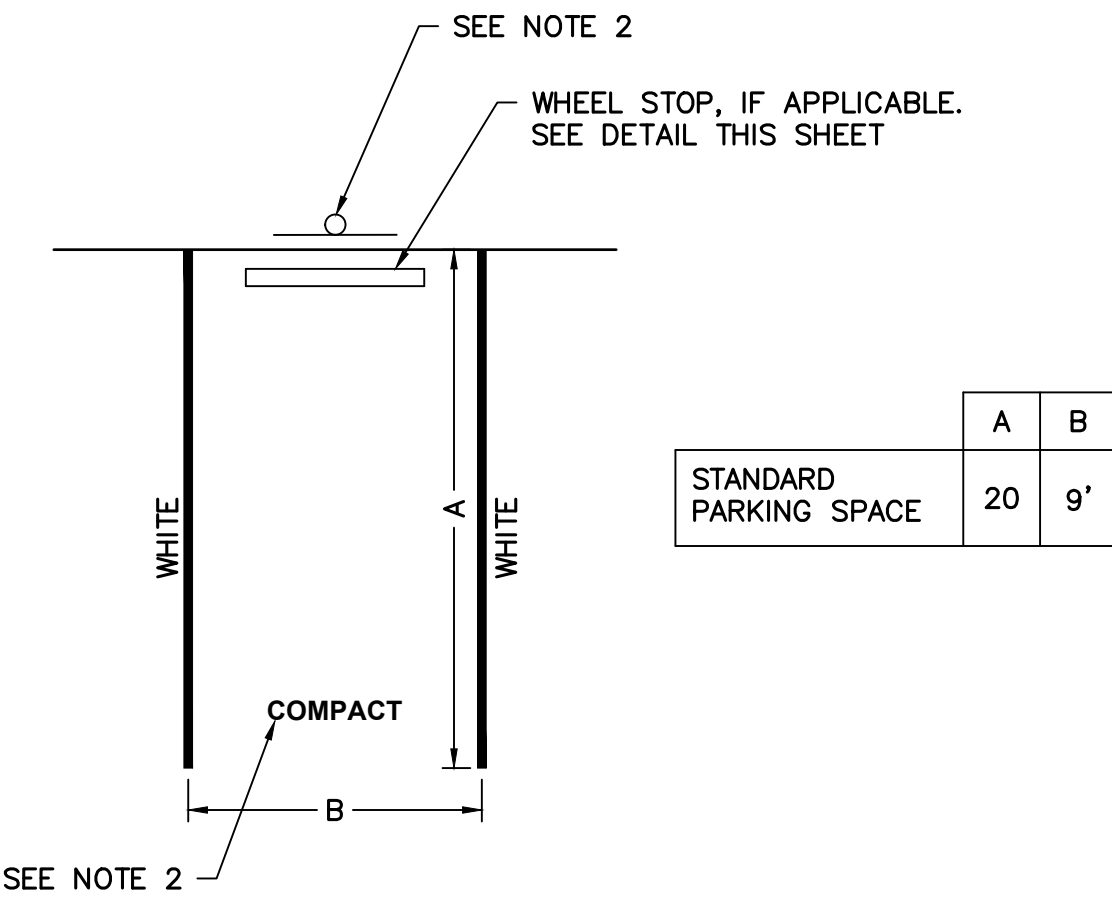
**SUBGRADE COMPACTION**  
N.T.S.



**TYPE "F" CURB DETAIL**  
N.T.S.

**NOTE:**

WHEN USED ON HIGH SIDE OF ROADWAYS, THE CROSS SLOPE OF THE GUTTER SHALL MATCH THE CROSS SLOPE OF THE ADJACENT PAVEMENT. THE THICKNESS OF THE LIP SHALL BE 6", UNLESS OTHERWISE SHOWN ON PLANS.



**NOTES:**

- EACH COMPACT-SIZE VEHICLE PARKING SPACE SHALL BE CLEARLY DESIGNATED BY EITHER PAVEMENT MARKING STATING "COMPACT" OR SIGNS STATING "PARKING FOR COMPACT VEHICLES ONLY."
- ALL PAVEMENT MARKINGS SHALL CONFORM TO FDOT STD. INDEXES 17346.

**TYPICAL PARKING STRIPING DETAIL**  
N.T.S.

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VIRGIL C. "LEE" LEWIS, P.E.  
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FBPR CERTIFICATE OF  
AUTHORIZATION NO. 5057

MISCELLANEOUS  
DETAILS (SHEET 2  
OF 2)  
60% DESIGN DRAWINGS

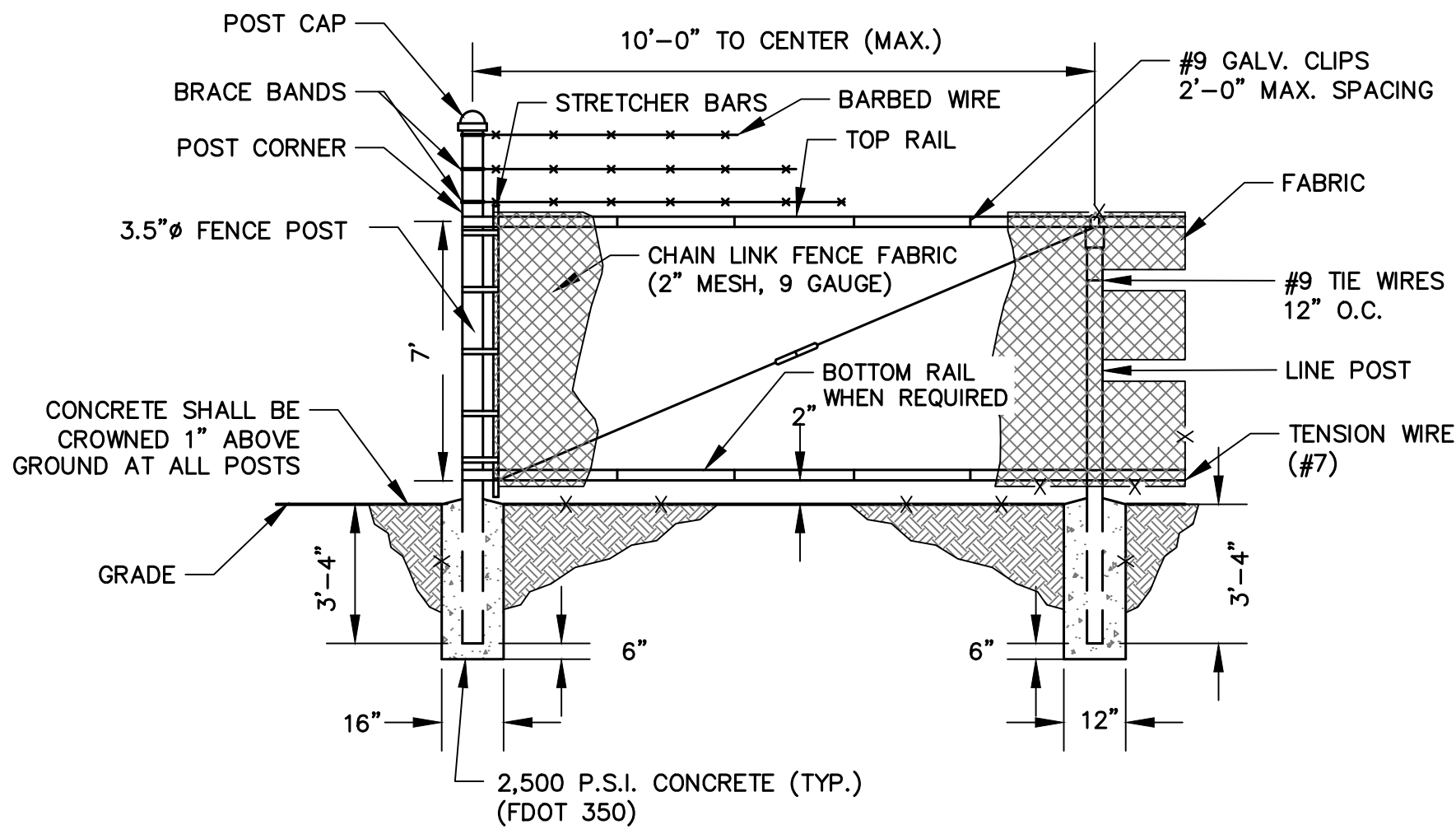
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IMPROVEMENTS  
PREPARED FOR  
TRI-COUNTY AIRPORT AUTHORITY

DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
DATE: FEBRUARY, 2021

SHEET NUMBER  
**C-07**

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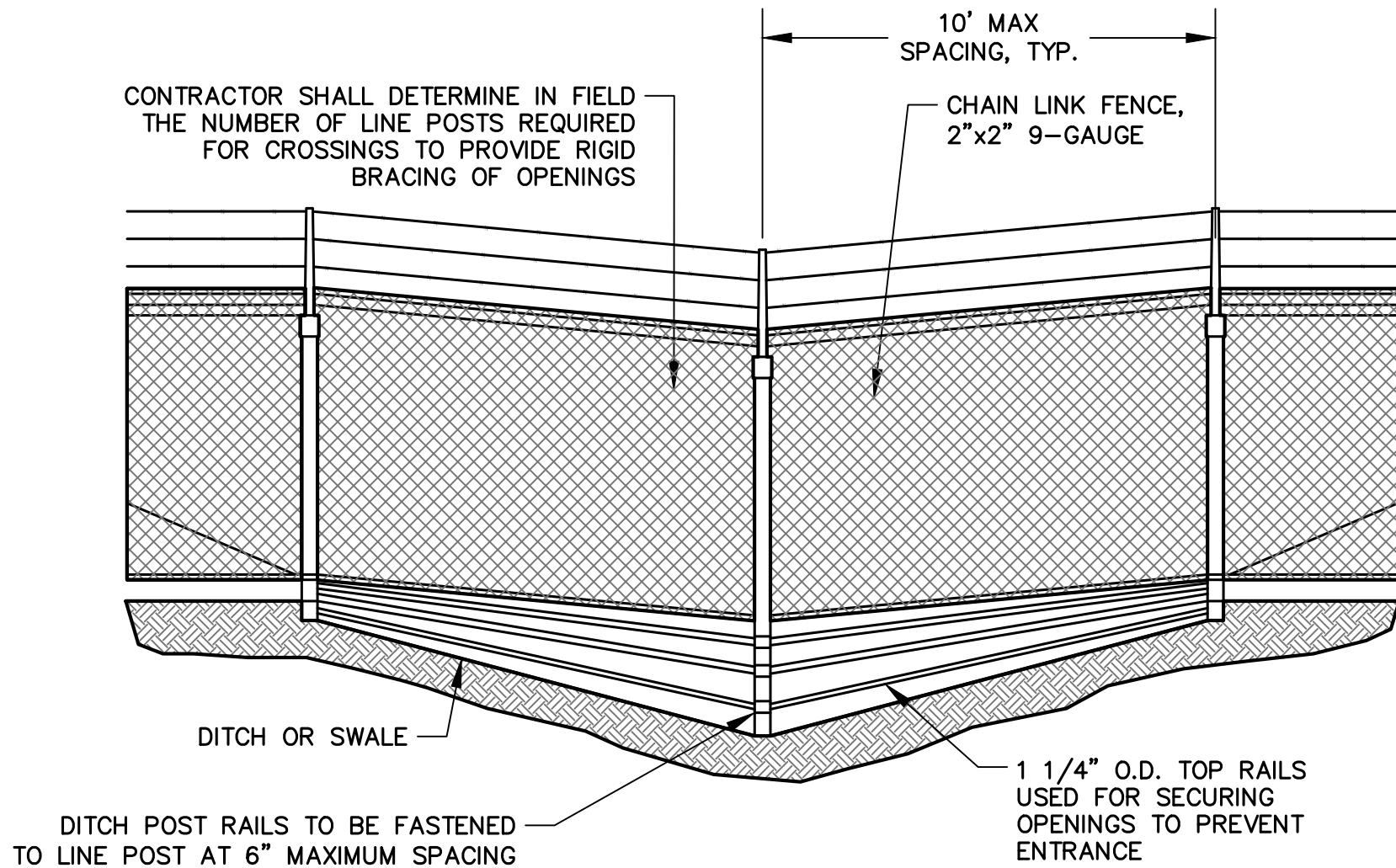


**7' WIRE MESH SECURITY FENCE, TYPE II**

N.T.S.

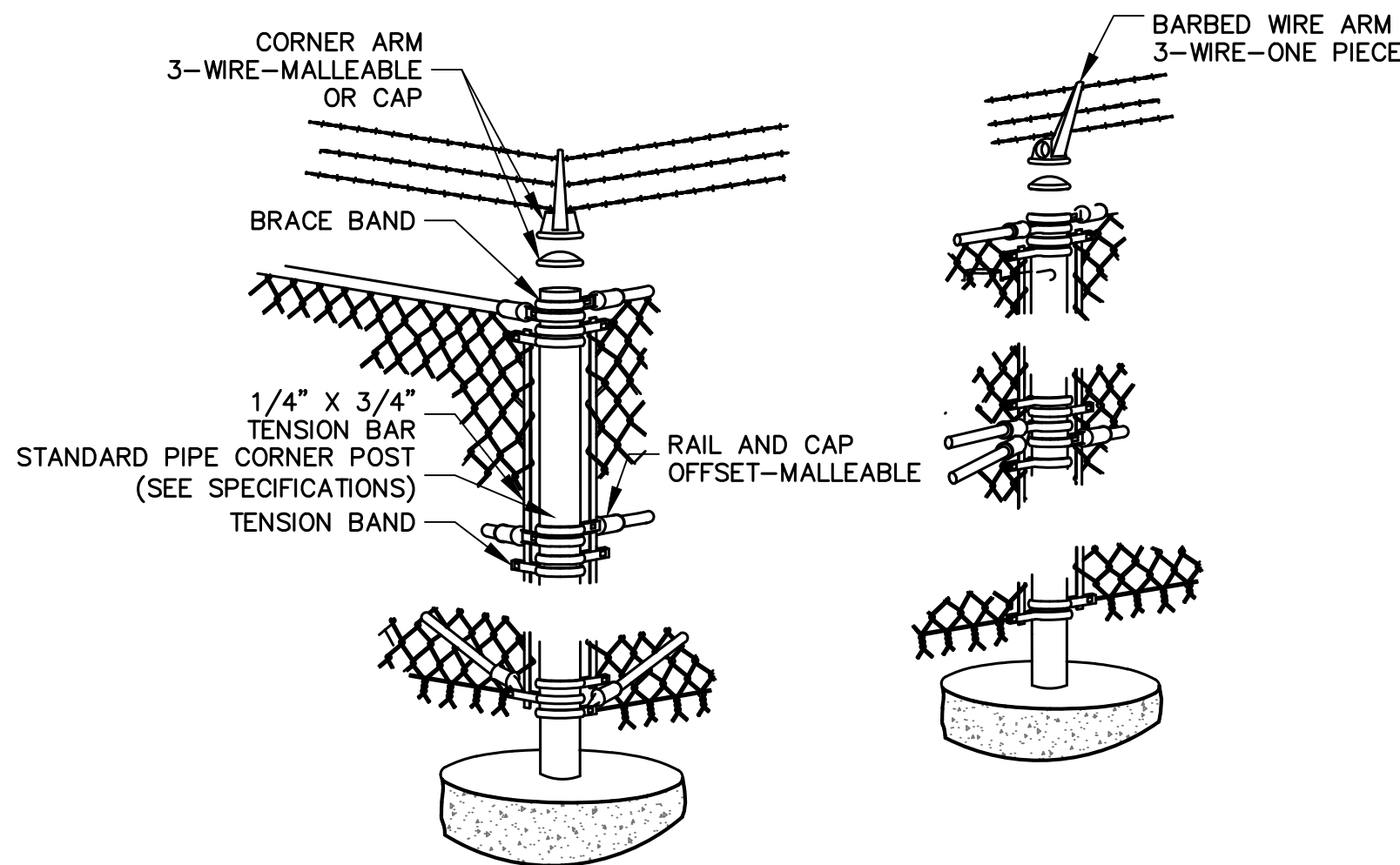
**NOTE:**

1. GALVANIZED STEEL LINE FENCE POST SHALL BE 2.25" IN DIAMETER.
2. GALVANIZED STEEL CORNER, PULL, AND END POSTS SHALL BE 3.5" IN DIAMETER. (PER FEDERAL SPEC. RR-F-191/3E)



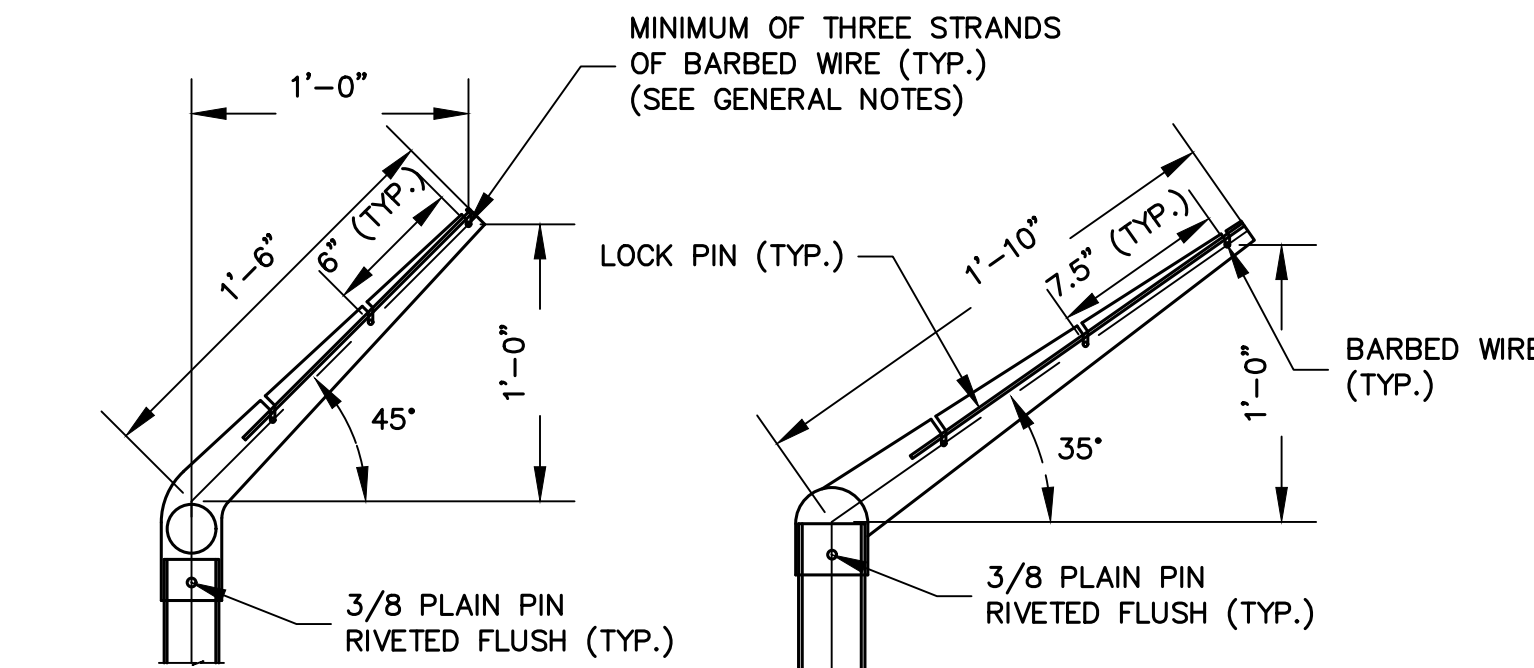
**DITCH SPAN**

N.T.S.



**CHAIN LINK FENCE ASSEMBLY**

N.T.S.

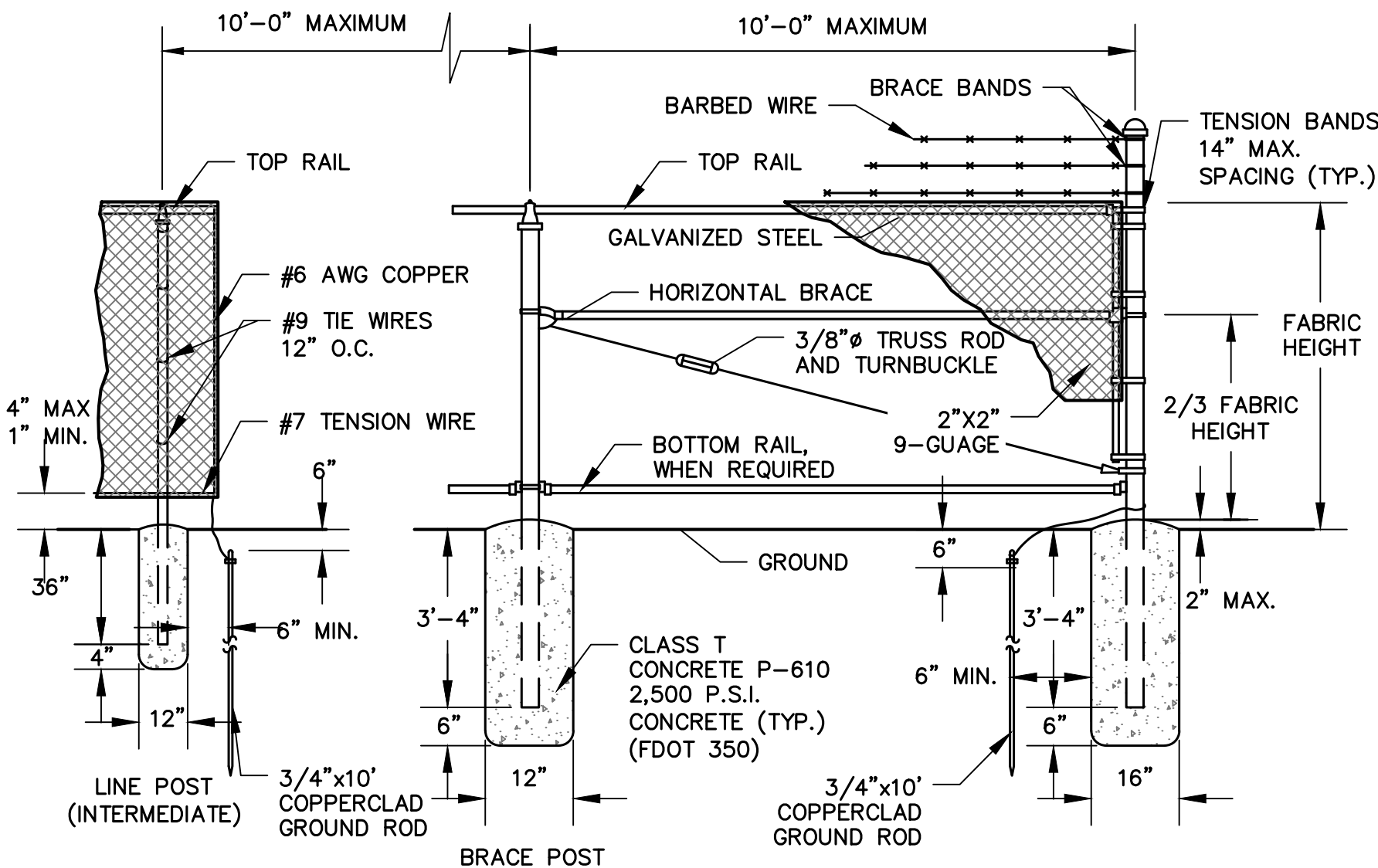


**NOTE:**

FENCE POST SHALL BE SET BACK SO BARBED WIRE DOES NOT EXTEND OVER PROPERTY LINES. BARBED WIRE ARMS SHALL BE INSTALLED ON SIDE AWAY FROM AIRPORT PROPERTY OR AIRCRAFT OPERATIONS AREA.

**BARBED WIRE EXTENSION ARM DETAILS**

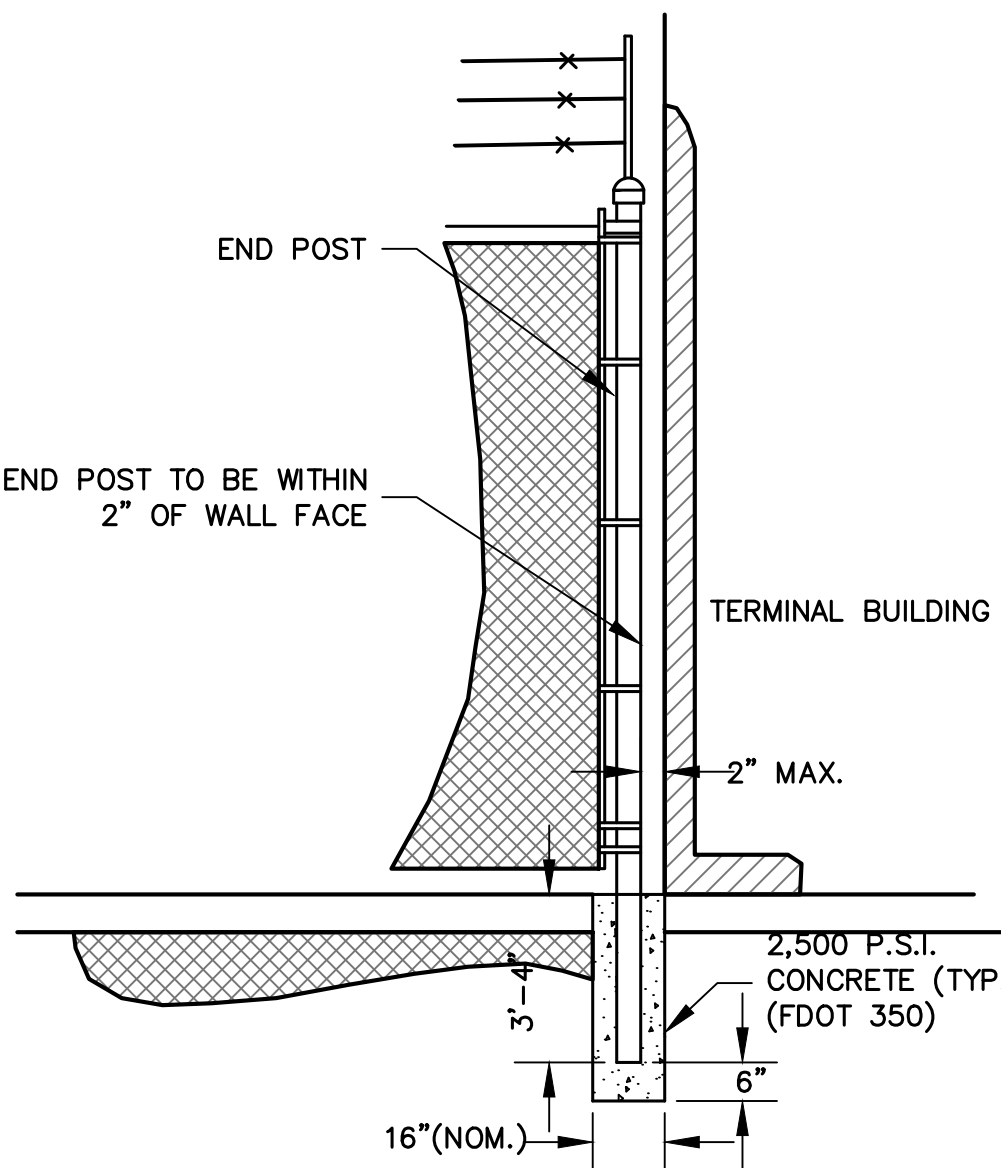
N.T.S.



NOTE:  
WHEN CONNECTING TO EXISTING FENCE,  
CONTRACTOR SHALL INSTALL A CORNER POST.

**BRACE SECTION CHAIN LINK FENCE**

N.T.S.



**WALL CONNECTION DETAIL**

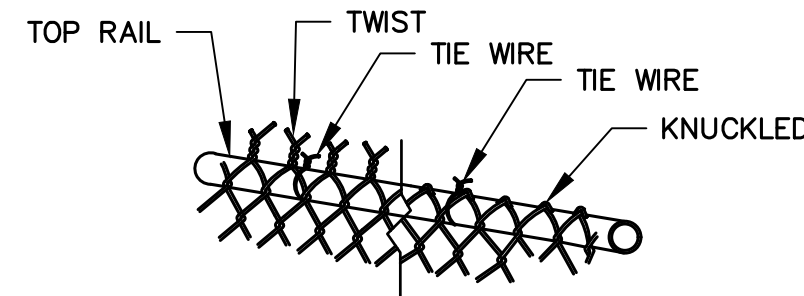
N.T.S.

**NOTES:**

1. CORE 12" THROUGH FOUNDATION/SLAB IF NECESSARY.
2. PLACE END POST NOT MORE THAN 2" FROM BUILDING PRIOR TO POURING FOOTING.
3. CONTRACTOR TO PATCH PAVEMENT SURFACE TO MATCH SURROUNDING PAVEMENT SURFACE AFTER REMOVAL.

**FENCE NOTES:**

1. ALL PIPE FRAMEWORK AND OTHER FERROUS MATERIALS SHALL BE HOT GALVANIZED, UNLESS OTHERWISE NOTED.
2. BOTTOM TENSION WIRE SHALL BE STRETCHED TAUT FROM TERMINAL POST TO TERMINAL POST AND SECURELY FASTENED TO EACH INTERMEDIATE POST 6 INCHES ABOVE GROUND LINE AND TO THE FABRIC CHAIN LINK.
3. PULL POSTS SHALL BE INSTALLED AT 500 FOOT INTERVALS ON STRAIGHT RUNS.
4. GATE POST ASSEMBLIES SHALL CONSIST OF A BRACE POST AND CORNER, END, OR PULL POST WITH BRACE AND TRUSS ROD ON EACH SIDE OF THE GATE.
5. CONNECTIONS TO EXISTING FENCES OR BUILDINGS SHALL BE MADE BY SETTING A NEW ANCHOR POST WITH BRACE ASSEMBLY AT JUNCTION POINT.
6. ALL FENCE AND GATES TO BE GROUNDED IN ACCORDANCE WITH THE SPECIFICATIONS.
7. DIMENSIONS: ALL DIMENSIONS, SIZES, GAUGES, WEIGHTS OR THICKNESS' SHOWN ARE THE MINIMUM ACCEPTABLE, UNLESS OTHERWISE INDICATED.
8. SPECIFICATIONS: MATERIALS AND CONSTRUCTION METHODS NOT DETAILED HEREON SHALL BE IN ACCORDANCE WITH THE FAA SPECIFICATION F-162 UNLESS OTHERWISE NOTED ON THE CONTRACT PLANS. FAA SPECIFICATIONS SHOWN ARE FROM THE FEDERAL AVIATION ADMINISTRATION STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS. THE FEDERAL SPECIFICATION "F-162" SHALL BE INTERPRETED TO MEAN THE LATEST ISSUE OR AMENDMENT OF SUCH SPECIFICATION IN EFFECT ON THE DATE OF PLAN APPROVAL.
9. MEASUREMENTS: FENCES ARE MEASURED IN PLACE, FROM CENTER TO CENTER OF END, CORNER OR GATE POSTS. MEASUREMENT DOES NOT INCLUDE GATE OPENINGS. GATES ARE MEASURED IN UNITS FOR EACH TYPE AND SIZE INSTALLED.
10. FABRIC INSTALLATION: WIRE OR FABRIC ON BOUNDARY AND SECURITY FENCES AND GATES SHALL BE ON THE SIDE OF POSTS AWAY FROM AOA.
11. PVC COATING: (NOT APPLICABLE)
12. CONCRETE: CONCRETE SHALL BE OF A COMMERCIAL GRADE WITH A MINIMUM 28 DAY STRENGTH OF 2500 P.S.I. FOOTING TOPS SHALL BE 1-INCH MINIMUM ABOVE GROUND AT ALL THE POSTS, AND TROWEL FINISHED TO SLOPE AWAY FROM THE POST.
13. OPENINGS UNDER FENCE: ANY OPENING UNDER FENCES, WHEREIN THE BOTTOM FENCE WIRE IS MORE THAN 4 INCHES ABOVE GROUND AND THE TOTAL AREA OF OPENING IS 96 SQUARE INCHES OR MORE, SHALL BE CLOSED USING THE DETAIL FOR SPANNING DITCHES OR APPROVED EQUAL. OPENINGS LESS THAN 18 INCHES HIGH SHALL BE CLOSED BY INSTALLING ONE OR MORE ADDITIONAL LINE POSTS BETWEEN THE OPENING CENTER AND ADJACENT LINE POSTS AT A 6-INCH MAXIMUM VERTICAL SPACING. THIS WORK SHALL BE INCIDENTAL TO FENCE INSTALLATION COSTS.
14. GROUNDING AND LIGHTNING PROTECTION, CHAIN LINK FENCE ONLY: EACH GATE LEAF FRAME SHALL BE CONNECTED TO THE GATE POST BY A BRAIDED, FLEXIBLE COPPER STRAP. EACH GATE POST SHALL BE GROUNDED. GROUND RODS SHALL BE PER SECTION F-162. GROUND CABLE SHALL BE NO. 2 AWG COPPER MINIMUM BARE STRANDED COPPER WIRE. CONNECTIONS TO GATE, FENCE, ETC. ABOVE GROUND SHALL BE MADE WITH SUITABLE NON-CORROSIVE METAL CLAMPS, LUGS OR CONNECTORS. CONNECTIONS TO GROUND RODS SHALL BE MADE BY THE EXOTHERMIC PROCESS. EACH ELEMENT OF THE FENCE SHALL BE GROUNDED. INSTALL GROUND RODS AND BONDING CONDUCTORS IN ACCORDANCE WITH F-162.
15. FENCE LINE AND ALIGNMENT: FENCE LINES SHALL BE CLEARED OF ALL OBSTRUCTIONS AND SMOOTH GRADED TO THE GENERAL CONTOUR OF THE ADJACENT GROUND. STUMPS AND ROOTS NOT INTERFERING WITH FENCE CONSTRUCTION, MAY BE CHIPPED TO GROUND LEVEL. THE FENCE SHALL BE CONSTRUCTED VERTICAL, STRAIGHT AND TRUE TO LINE. THE LONGITUDINAL GRADIENT SHALL PARALLEL TO THE GENERAL SLOPE OF THE GROUND. CONTRACTOR SHALL LAY OUT THE FENCE ALIGNMENT AS SHOWN ON THE CONSTRUCTION PLANS. THE ENGINEER SHALL APPROVE THE ALIGNMENT, LINE AND GRADE OF THE FENCE AND THE GATE LOCATION(S) PRIOR TO CONSTRUCTION.
16. AIRPORT SECURITY FENCES AND GATES SHALL INCLUDE BARB WIRE ATTACHMENT. BARB WIRE ATTACHMENTS SHALL EXTEND IN THE DIRECTION AWAY FROM THE AIRFIELD.
17. THE CAP ARM SHALL BE DESIGNED TO PROVIDE A DRIVE FIT OVER THE TOP OF POSTS AND TO EXCLUDE MOISTURE IN POSTS WITH TUBULAR SECTIONS.
18. GATES SHALL BE INSTALLED PLUMB, LEVEL, AND SECURE, WITH FULL OPENING WITHOUT INTERFERENCE. GROUND-SET ITEMS SHALL BE INSTALLED IN CONCRETE FOR ANCHORAGE. HARDWARE SHALL BE ADJUSTED FOR SMOOTH OPERATION.
19. PERIMETER GATE FRAMES SHALL BE FABRICATED OR TUBULAR MEMBERS. ADDITIONAL HORIZONTAL AND VERTICAL MEMBERS SHALL BE PROVIDED AS REQUIRED TO ENSURE PROPER GATE OPERATION AND FOR ATTACHMENT OF FABRIC AND HARDWARE. SIZES OF FRAME MEMBERS LISTED ARE MINIMUM; LARGER SIZES SHALL BE PROVIDED AS REQUIRED.
20. GATE FRAME ASSEMBLY SHALL BE WELDED OR ASSEMBLED WITH SPECIAL MALLEABLE OR PRESSED STEEL FITTINGS AND RIVETS TO PROVIDE RIGID CONNECTIONS. FABRIC SHALL BE INSTALLED WITH STRETCHER BARS AT VERTICAL EDGES. STRETCHER BARS MAY ALSO BE USED AT THE TOP AND BOTTOM EDGES. STRETCHER BARS AND FABRIC SHALL BE ATTACHED TO GATE FRAMES ON ALL SIDES AT INTERVALS NOT EXCEEDING 15 INCHES. HARDWARE SHALL BE ATTACHED WITH RIVETS OR BY OTHER MEANS THAT WILL PROVIDE EQUAL SECURITY AGAINST BREAKAGE OR REMOVAL.
21. WHERE BARBED WIRE IS INDICATED ABOVE GATES, THE END MEMBERS OF GATE FRAMES SHALL BE EXTENDED APPROXIMATELY ONE (1) FOOT ABOVE THE TOP MEMBER WITH PROVISION FOR ATTACHING THE WIRE. VERTICAL SUPPORT ARMS SHALL BE PROVIDED AT INTERMEDIATE POINTS, WITH SPACING TO MATCH THE SPACING OF THE LINE POSTS.
22. DIAGONAL CROSS-BRACING SHALL BE PROVIDED, CONSISTING OF 3/8 INCH DIAMETER ADJUSTABLE LENGTH TRUSS RODS ON WELDED GATE FRAMES WHERE NECESSARY TO OBTAIN FRAME RIGIDITY WITHOUT SAG OR TWIST. NON-WELDED GATE FRAMES SHALL HAVE DIAGONAL BRACING.



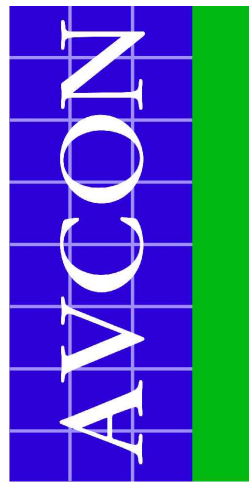
**FABRIC SELVAGE**

N.T.S.

NOTE:  
CONTRACTOR SHALL USE THE KNUCKLED SYSTEM FOR FABRIC SELVAGE.

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FBPR CERTIFICATE OF  
AUTHORIZATION NO. 5057

FENCE DETAILS  
(SHEET 1 OF 2)

60% DESIGN DRAWINGS

TERMINAL ACCESS  
& SECURITY  
IMPROVEMENTS

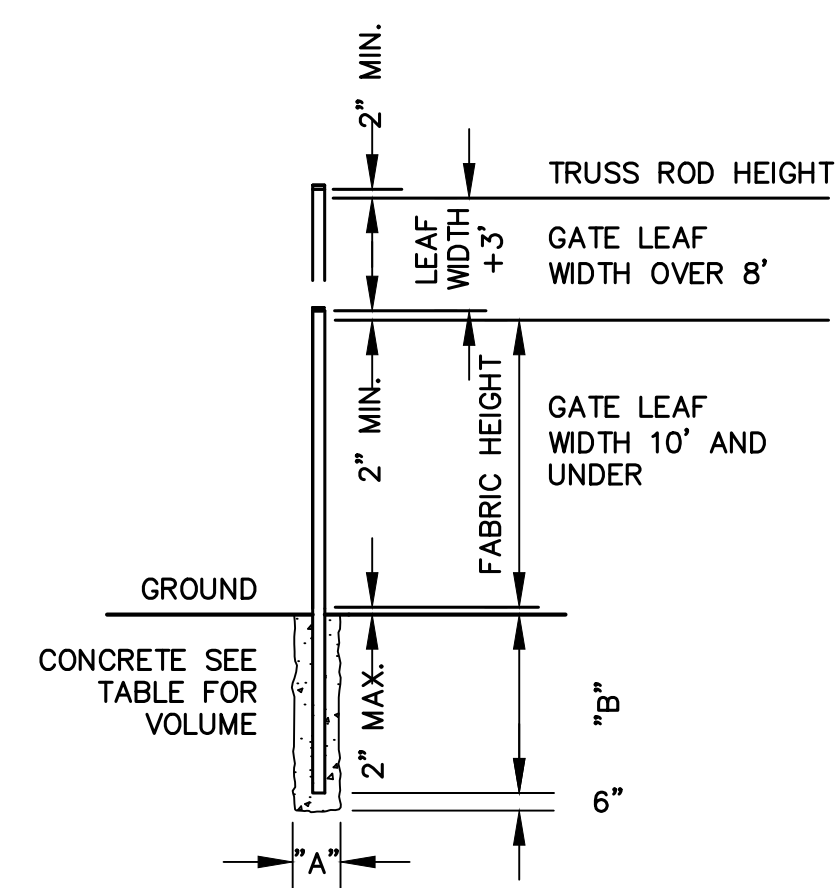
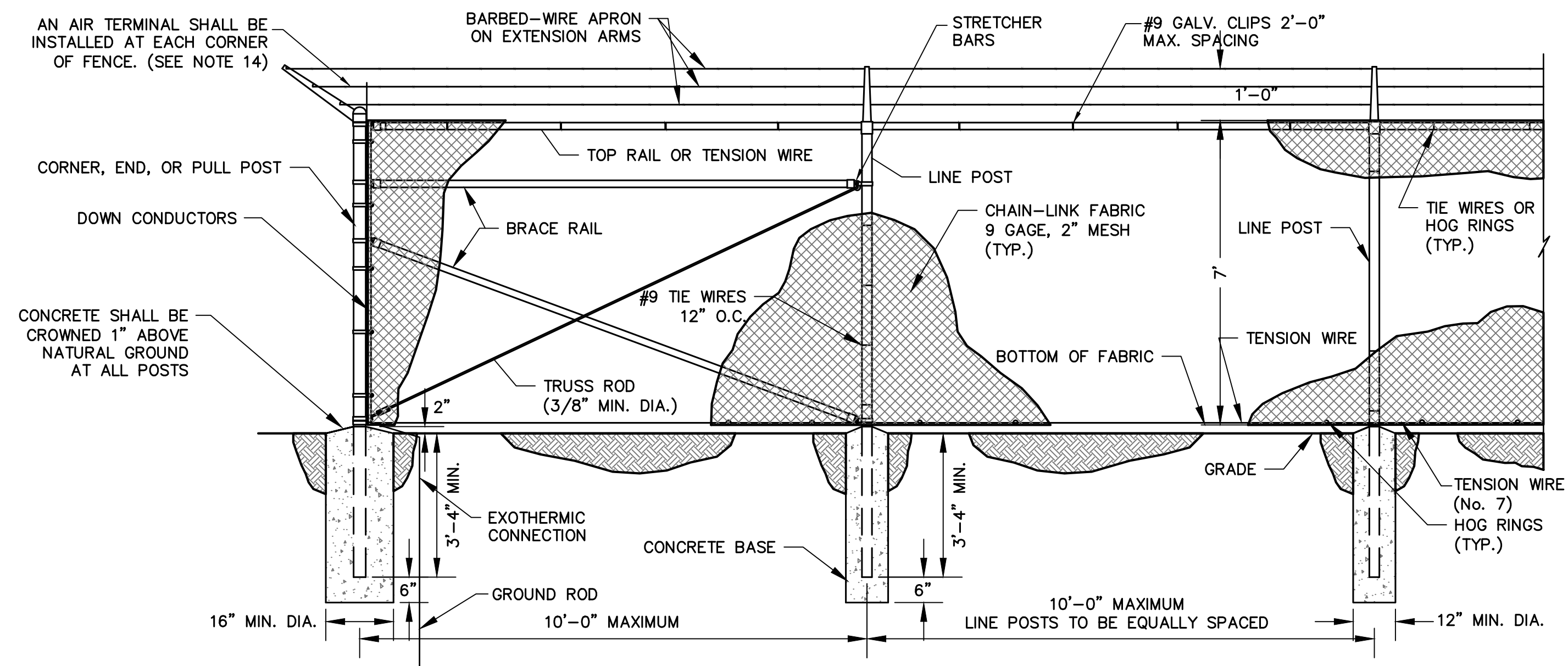
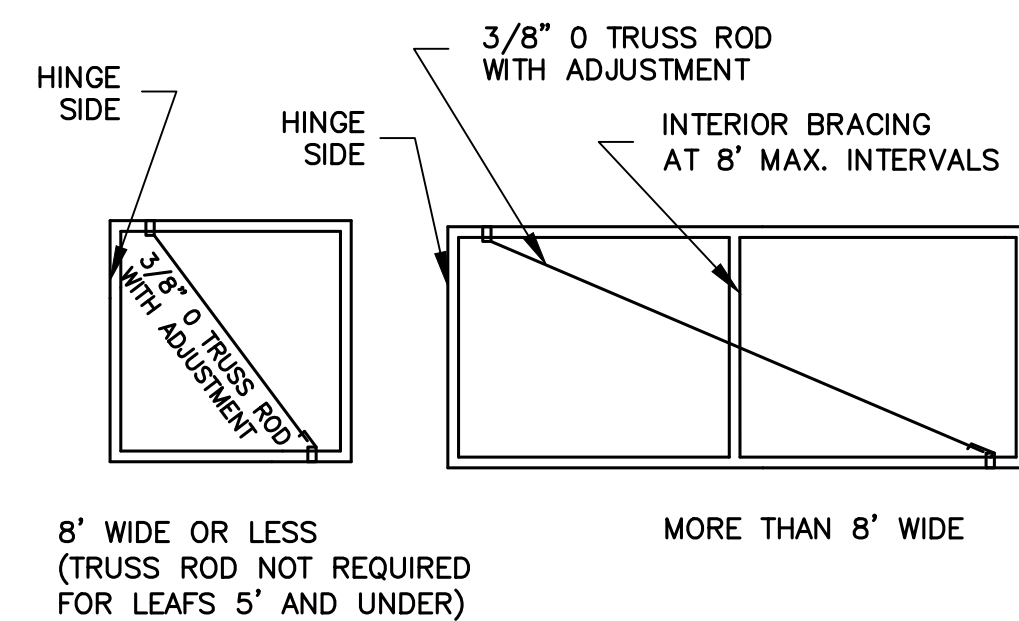
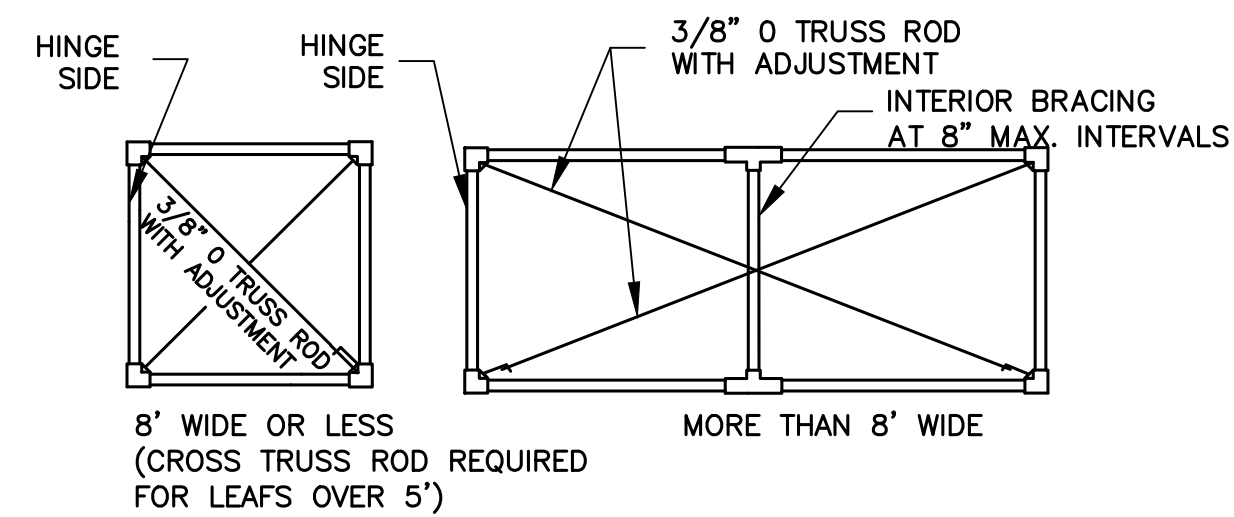
PREPARED FOR  
TRI-COUNTY AIRPORT AUTHORITY

DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
DATE: FEBRUARY, 2021

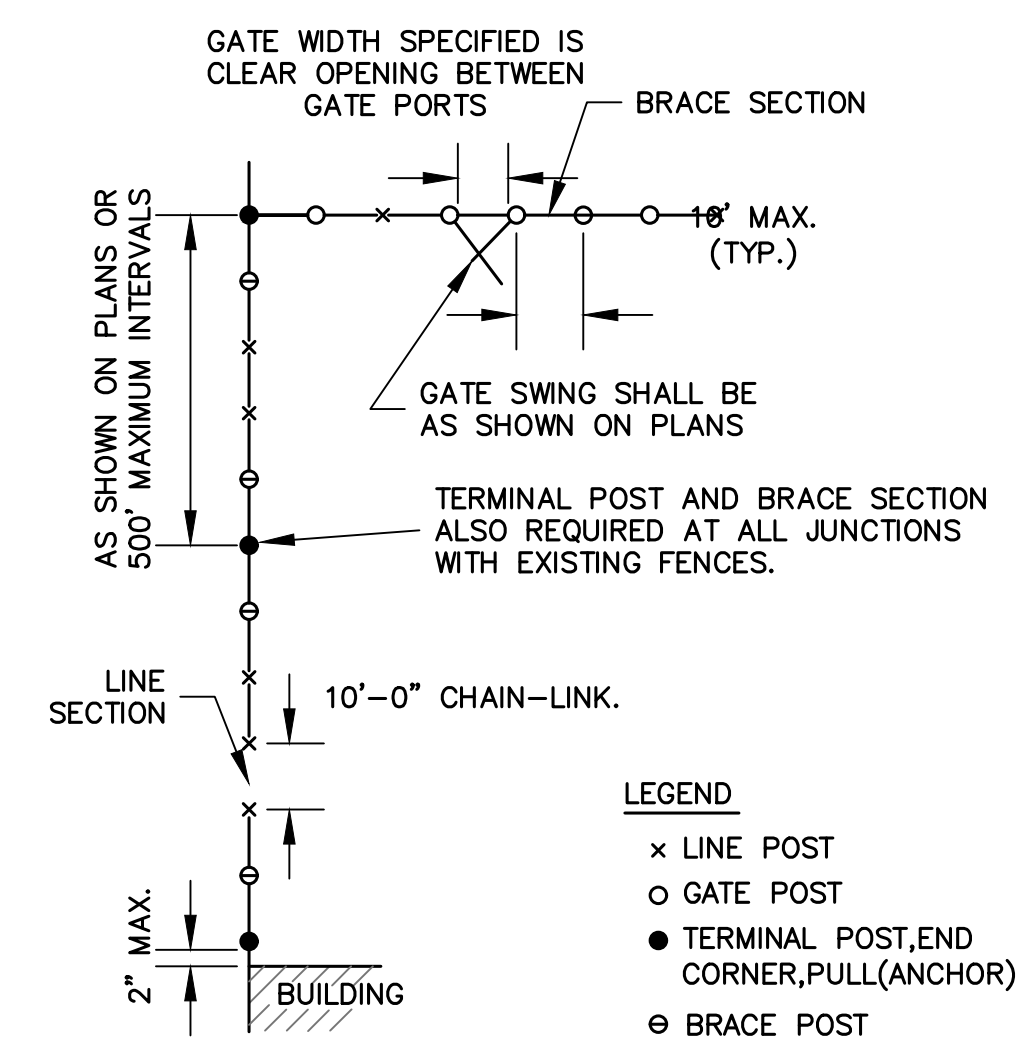
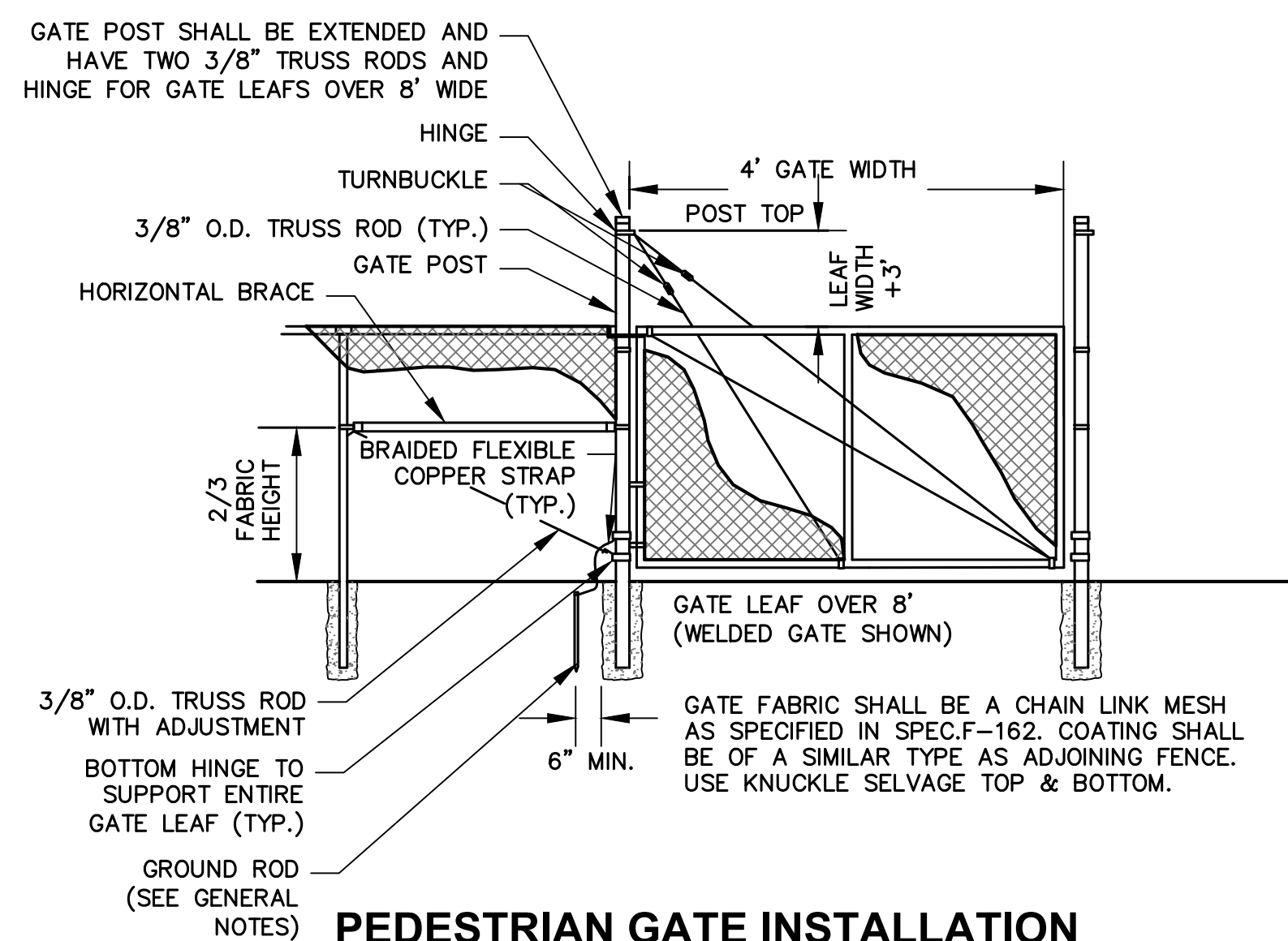
SHEET NUMBER

C-08





DIMENSIONS	GATE WIDTH (ONE LEAF)			
	6' AND LESS	OVER 6' THROUGH 10'	OVER 10' THROUGH 15'	OVER 15' THROUGH 18'
A	12"	16"	20"	24"
B	48"	56"	62"	68"
C.Y. CONC.	0.13	0.26	0.45	0.70





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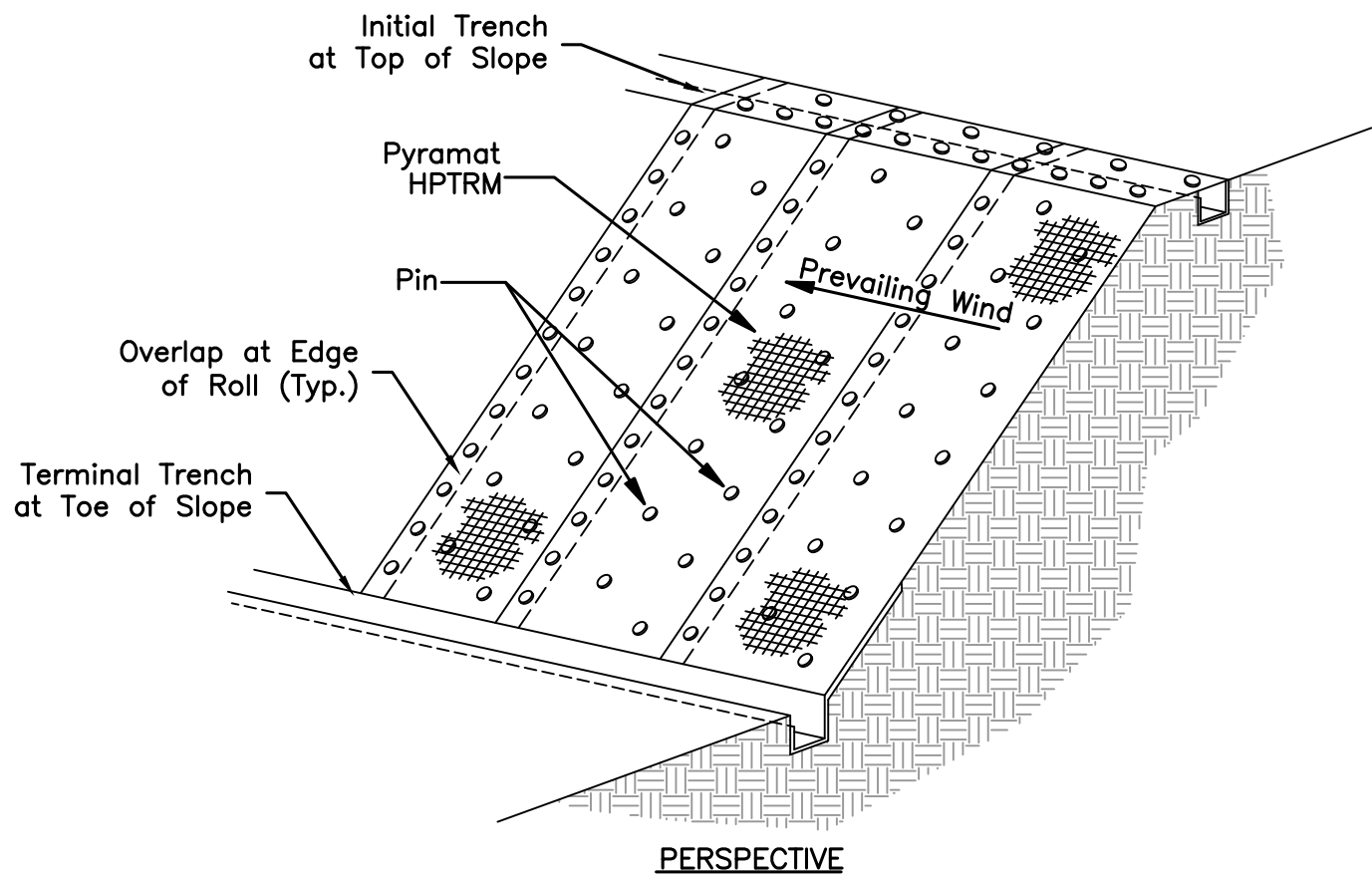


FIGURE 1: OVERVIEW OF  
PYRAMAT® HPTRM ON SLOPE

\*Note: For Clarity, Perspective View does not show all pins

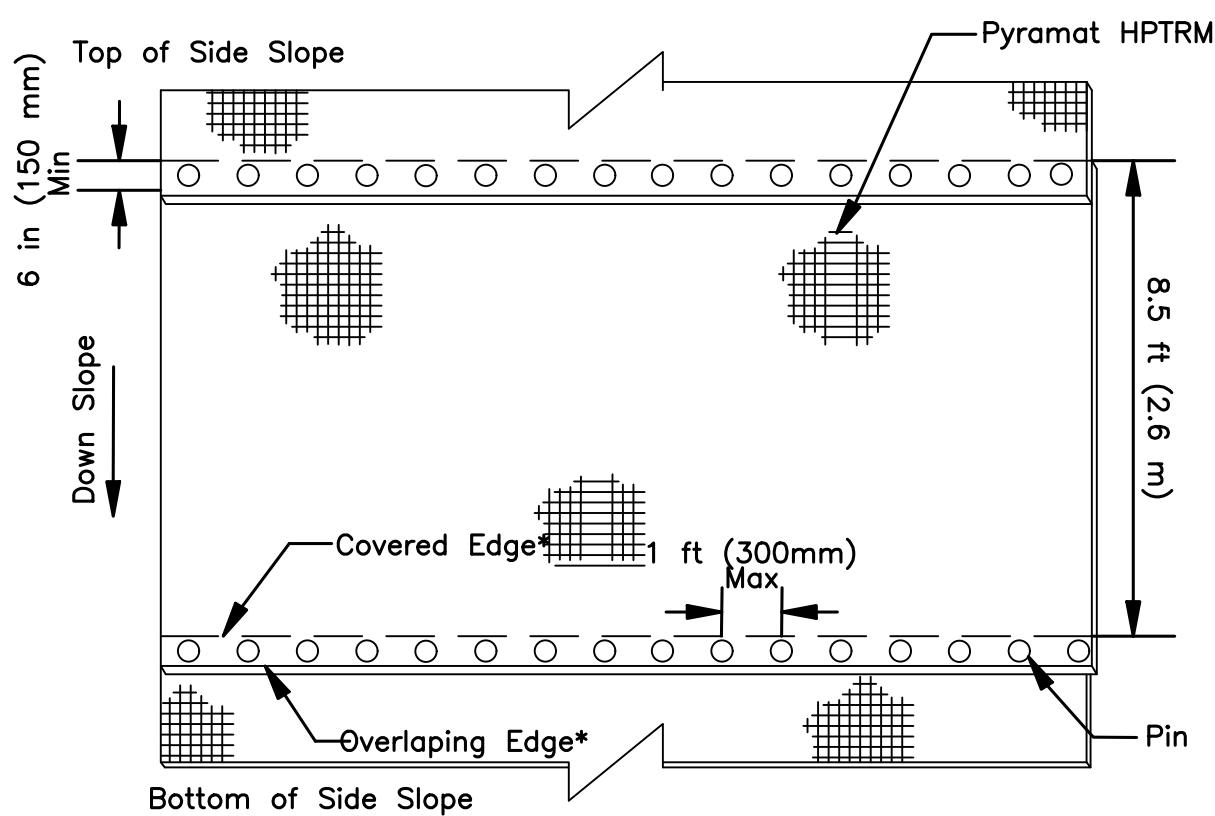


FIGURE 4: PYRAMAT® HPTRM  
OVERLAP AT ROLL EDGE  
DETAIL

\*Note: Pyramat HPTRM Edge Shingle/Overlap placement depends on down slope direction (i.e Shingle in the direction of the down slope)

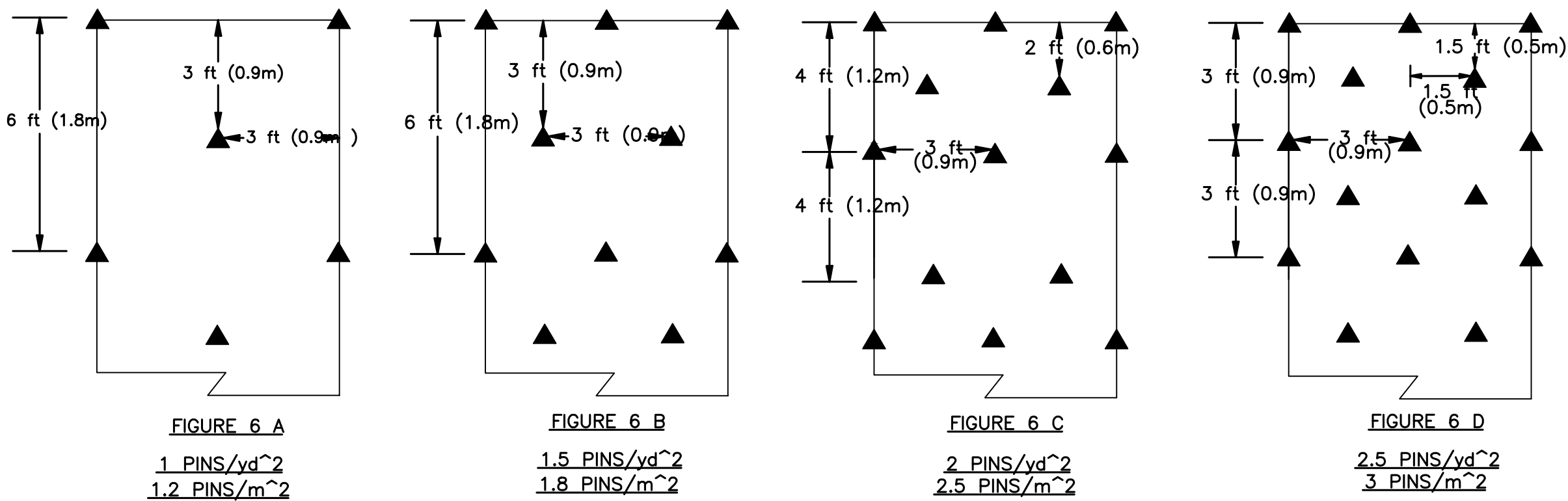


FIGURE 6: PIN PATTERN DETAIL

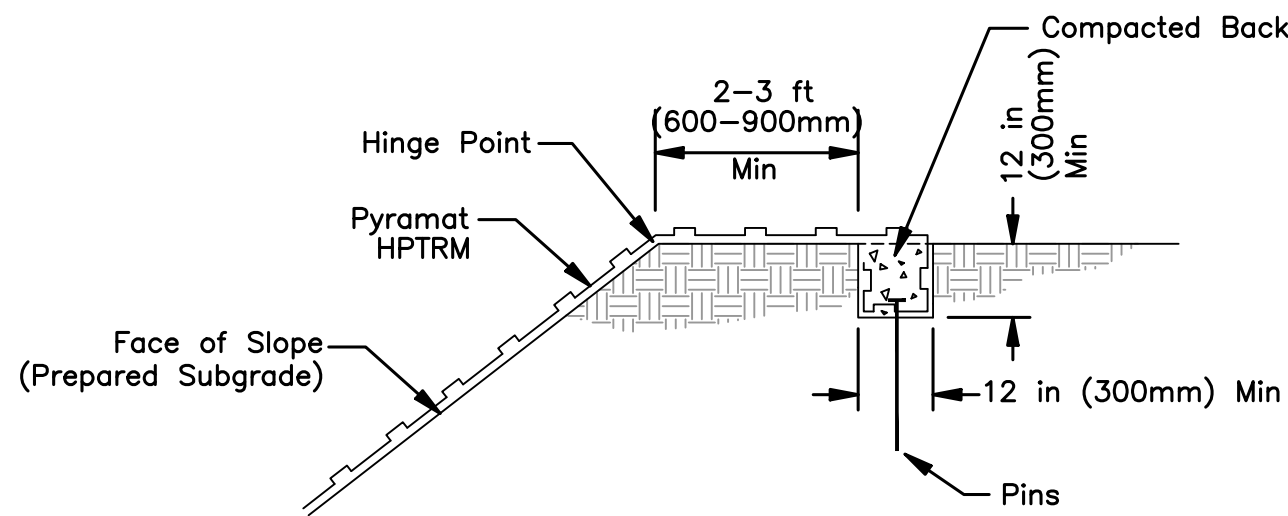


FIGURE 2: INITIAL  
TRENCH AT TOP OF  
SLOPE DETAIL

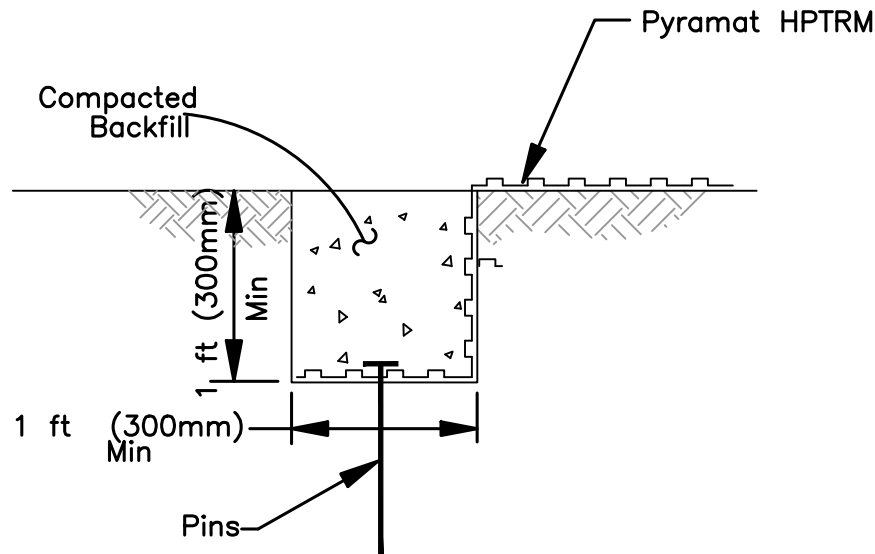


FIGURE 3:  
LONGITUDINAL EDGE  
TRENCH DETAIL

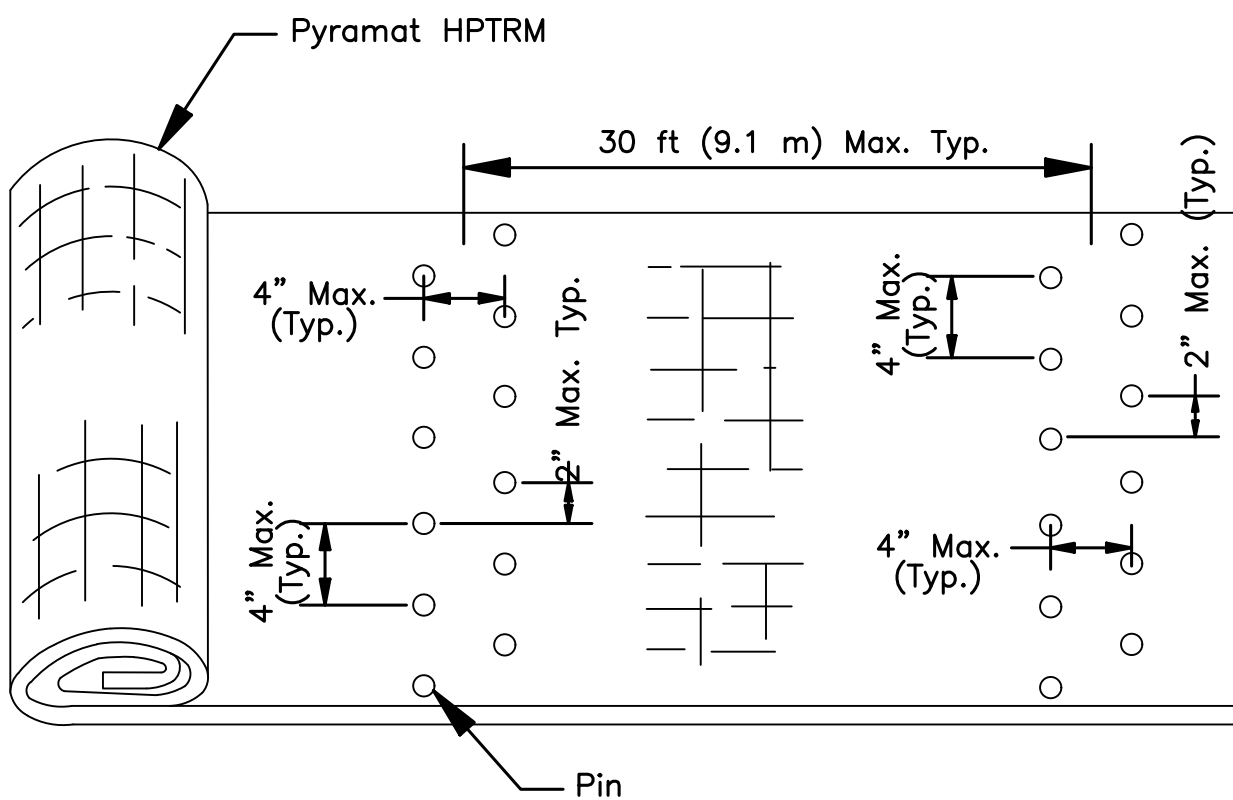


FIGURE 7: SIMULATED CHECK SLOT DETAIL

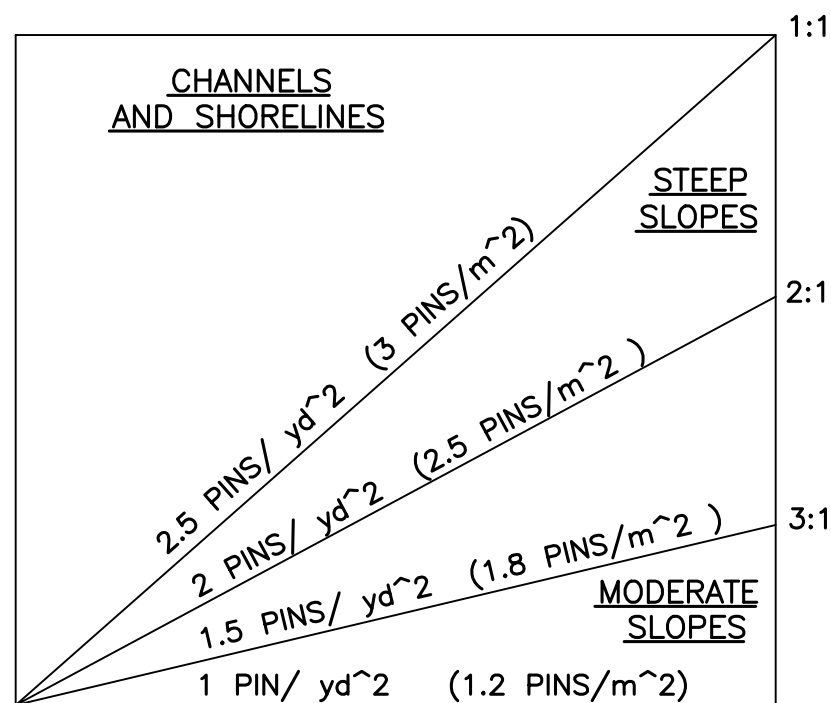


FIGURE 6 E  
PIN PATTERN GUIDE

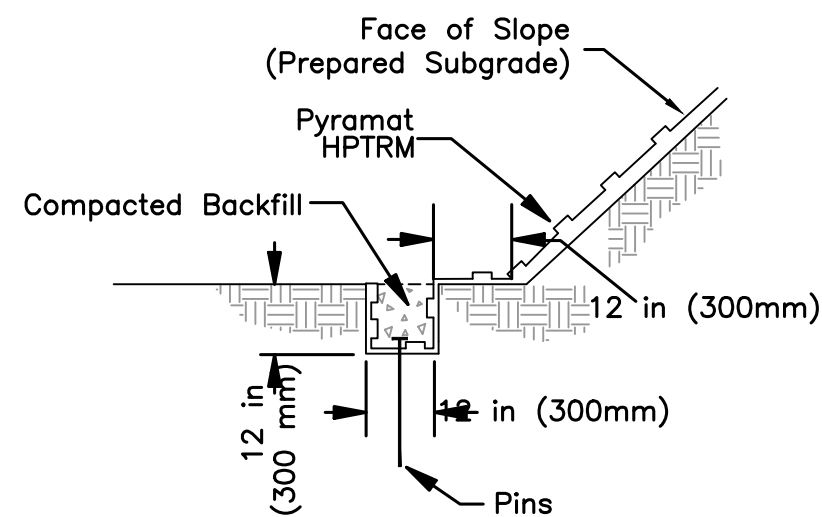


FIGURE 8: TERMINAL  
TRENCH AT TOE OF SLOPE  
DETAIL

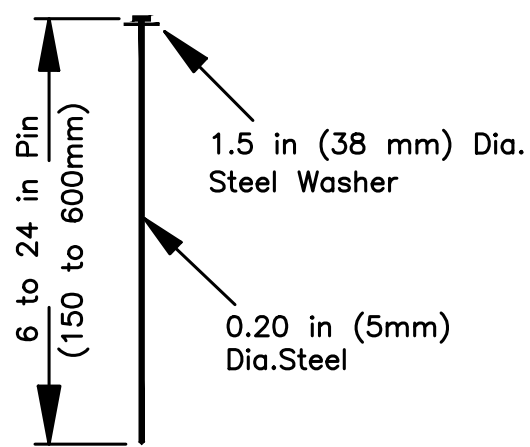


FIGURE 9: PIN DETAIL

## PYRAMAT® HPTRM ON A SLOPE

### PRE-CONSTRUCTION

- A pre-construction meeting should be held with the construction team and a representative from Propex. This meeting shall be scheduled by the contractor with at least two weeks notice. Also, Propex suggests that installation monitoring of the Pyramat→HPTRM be performed by a qualified independent third party.

### SITE PREPARATION

- Grade and compact area of Pyramat→HPTRM installation as directed and approved by Engineer. Subgrade shall be uniform and smooth. Remove all rocks, clods, vegetation or other objects so the installed mat will have direct contact with soil surface.
- Prepare seedbed by loosening the top 2-3 in (50-75 mm) minimum of soil.
- Incorporate amendments such as lime and fertilizer and/or wet the soil, if needed.
- Do not mulch areas where mat is to be placed.

### SEEDING

- Apply seed to soil surface before installing mat. Disturbed areas shall be reseeded.
- Consult project plans and/or specifications for seed types and application rates.

### INSTALLATION ON A SLOPE

- Figure 1 shows general installation layout and details for Pyramat→HPTRM on a slope.
- Excavate initial trench 12 in wide x 12 in deep (300 mm x 300 mm) minimum 2-3 ft (600-900 mm) over crest of slope (see Figure 2).
- Excavate longitudinal edge trenches 12 in wide x 12 in deep (300 mm x 300 mm) minimum along both sides of the installation to bury edges of mat (see Figure 3). The longitudinal edge trenches shall be located along the first and last installed rolls.
- Beginning at the downstream end of prevailing winds, place roll end into the initial trench and secure with pinning devices on 12 in (300 mm) centers (see Figure 2). Position adjacent rolls and secure in trench in same manner. Backfill and compact soil into trench as directed and approved by Engineer.
- Unroll mat down the slope over the compacted initial trench.
- Secure longitudinal edge trench with pinning devices on 12 in (300 mm) centers (see Figure 3).
- Continue installation as described above, overlapping adjacent rolls as follows:
  - Roll edge overlap: 6 in (150 mm) minimum overlap with upstream mat on top. Secure with one row of ground pinning devices on 12 in (300 mm) centers (see Figure 4).
  - Roll end overlap: 12 in (300 mm) minimum overlap with upslope mat on top. Secure with two rows of ground pinning devices staggered 12 in (300 mm) apart on 12 in (300 mm) centers (see Figure 5).
- Secure mat using suggested ground pinning devices for appropriate frequency and pattern shown on the Pin Pattern Guide (see Figure 6).
- For slope heights greater than 45 ft (13.7 m), install simulated check slots per Figure 7. This method includes placing two staggered rows of pins on 4 in (100 mm) centers at 30 ft (9.1 m) intervals (see Figure 7) or across the midpoint of the slope height for slope heights less than 60 ft (18.2 m).
- Excavate terminal trench 12 in wide by 12 in deep (300 mm x 300 mm) minimum 12 in (300 mm) from the toe of slope (see Figure 8). Terminal trench pinning devices should be spaced on 12 in (300 mm) centers (see Figure 8).

### GROUND PINNING DEVICES

- Metal pins should be at least 0.20 in (5 mm) diameter steel with a 1 1/2 in (38 mm) steel washer at the head of the pin (see Figure 9). Metal pins should be driven flush to the soil surface. Pins should be between 6-24 in (150-600 mm) long and have sufficient ground penetration to resist pullout. Longer pins may be required for looser soils. Heavier metal stakes may be required in rocky soils. Depending on soil pH and design life of the pin, galvanized or stainless steel pins may be required. Consult project plans and/or specifications for tie down device details.

### SPECIAL TRANSITIONS

- For applications that require special transitions (i.e. connections to riprap, concrete, headwalls, etc.), refer to Page 6 of the Installation Guidelines for Landlok→TRMs and Pyramat→HPTRMs.

### SOIL FILLING

- Installed Pyramat→HPTRM shall be re-seeded and soil-filled.
- After seeding, spread and lightly rake 1/2 - 3/4 in (12-19 mm) of fine site soil or topsoil into the mat and completely fill the voids using backside of rake or other flat tool.
- If equipment must operate on the mat, make sure it is of the rubber-tired type. No tracked equipment or sharp turns are allowed on the mat.
- Avoid any traffic over the mat if loose or wet soil conditions exist.
- Smooth soil-fill in order to just expose the top of Pyramat→HPTRM. Do not place excessive soil above the mat.
- Broadcast additional seed and install a Landlok→ECB above the soil-filled mat (if desired).
- Irrigate as necessary to establish/maintain vegetation. Do not over irrigate.

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FBPR CERTIFICATE OF  
AUTHORIZATION NO. 5057

NO.	DATE	REVISION	BY

## PYRAMAT DETAILS

60% DESIGN DRAWINGS

## TERMINAL ACCESS & SECURITY IMPROVEMENTS

PREPARED FOR  
TRI-COUNTY AIRPORT AUTHORITY

DESIGNED BY: T.N./C.P.  
DRAWN BY: M.B.  
CHECKED BY: T.N.  
APPROVED BY: V.L.  
PROJECT NO: 2020.268.01  
DATE: FEBRUARY, 2021

SHEET NUMBER

C-11

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